



KANSAS CITY
MISSOURI

October 2007



RESOLUTION NO. 070976, AS AMENDED

Approving the Knobtown Land Use and Development Plan as a guide for the future development of the area generally bounded by 75th Street/Gregory Boulevard on the north, the Raytown City Limits/Westridge Road on the west, Bannister Road on the south and Rhinehart Road on the east; designating the Knobtown Land Use and Development Plan as the plan of record for this area; and repealing and replacing those portions of the Little Blue Valley Area Plan and the Longview Area Plan which are within the area described above. (664-S, 406-S-15, 253-S-8).

WHEREAS, the stakeholders of the Knobtown Area in conjunction with the City Development Department have conducted a study delineating the characteristics of the Knobtown Area; and

WHEREAS, the Knobtown Land Use and Development Plan has incorporated the policies of the FOCUS Kansas City Plan and other appropriate planning documents; and

WHEREAS, the policies within the Knobtown Land Use and Development Plan, which will serve as the plan of record for this area, are consistent with the policies of the FOCUS Kansas City Plan, and any future amendments to the Knobtown Land Use and Development Plan should be guided by and comply with the policy direction set forth in the FOCUS Kansas City Plan, adopted by Resolution No. 971268 in 1997; and

WHEREAS, as a result of said study, the City Development Department has proposed a framework for the orderly development within the Knobtown Area; and

WHEREAS, said framework provides a guide for future development and redevelopment of the area, for specific and general policies to guide future decisions, and for identification of public and private needs throughout the community and possible solutions to those needs; and

WHEREAS, the City Development Department has incorporated its findings in the Knobtown Land Use and Development Plan; and

WHEREAS, the Knobtown Land Use and Development Plan and proposed amendments to existing plans were unanimously approved by the City Plan Commission on August 7, 2007 (Case Nos. 664-S, 406-S-15 and 253-S-8); NOW, THEREFORE,

BE IT RESOLVED BY THE COUNCIL OF KANSAS CITY:

RESOLUTION NO. 070976, AS AMENDED

Section A. That the Knobtown Land Use and Development Plan, as amended on October 4, 2007, is hereby adopted as a guide for the future development public investments for that area generally bounded by 75th Street/Gregory Boulevard on the north, the Raytown City Limits/Westridge Road on the west, Bannister Road on the south and Rhinehart Road on the east. A copy of the Knobtown Land Use and Development Plan is attached hereto, in substantial form, and is hereby approved.

Section B. That the Knobtown Land Use and Development Plan, as amended on October 4, 2007, is consistent and complies with the FOCUS Kansas City Plan, adopted on October 30, 1997, by Committee Substitute for Resolution No. 971268, and is adopted as a supplement to the FOCUS Kansas City Plan.

Section C. That the Council finds and declares that before taking any action on the proposed Knobtown Land Use and Development Plan hereinabove, all public notices have been given and hearings have been held as required by law.



Authenticated as Passed:


Mark Funkhouser, Mayor


Millie M. Crossland, City Clerk

OCT 04 2007
Date Passed

CREDITS and acknowledgments

MAYOR:

The Honorable Mark Funkhouser

CITY COUNCIL:

Bill Skaggs..... 1st District
Deb Hermann..... 1st District-at-Large
Russ Johnson..... 2nd District
Ed Ford..... 2nd District-at-Large
Sharon Sanders Brooks..... 3rd District
Melba Curls..... 3rd District-at-Large
Jan Marcason..... 4th District
Beth Gottstein..... 4th District-at-Large
Terry Riley..... 5th District
Cindy Circo..... 5th District-at-Large
John A. Sharp..... 6th District
Cathy Jolly..... 6th District-at-Large

CITY MANAGER:

Wayne A. Cauthen



STEERING COMMITTEE:

Councilwoman Cindy Circo.....City Council
Councilman Terry Riley.....City Council
Ernest Wassmann (Co-Chair).....Silvertooth Subdivision
Dan Porrevecchio (Co-Chair).....Little Blue Valley Homes Association
Councilwoman Becky Schumacher.....City Council
Kevin Thomas.....Team Excavating, LLC
Jeff Martin.....Kansas City International Raceway
Jeff Vance.....Teetering Rocks Golf Course
Phil Jones.....The Villas at Teetering Rocks
Douglas Axon.....Unity Ridge Property Owners Association
Carl Vaitl.....High View Estates Neighborhood
Bill Hillix.....Cedar Valley Subdivision
Dusty Dahmer.....Boggs Valley Subdivision
Terry Eakins.....Knobtown Business/Property Owner
Phillip Giarratano.....Knobtown Business/Property Owner
Roland Obrien.....Knobtown Business/Property Owner
Gary Watts.....Knobtown Business/Property Owner
Steven Williams.....Knobtown Business/Property Owner
Pete Vegelahn.....Vegelahn Construction, LLC
Kevin Thomas.....Public Improvements Advisory Committee
David Whalen.....Public Improvements Advisory Committee
Dale Houck.....Raytown School District
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SUMMARY

Purpose and Goals for the Plan

THE KNOBTOWN DEVELOPMENT PLAN PROVIDES:

- A policy framework to strategically guide decisions regarding land use and development, transportation, and public investments.
- A single, coordinated Plan for the area which promotes the citywide initiatives established in the City's Comprehensive Plan (the FOCUS Kansas City Plan) the Major Street Plan, the KC-ONE Stormwater Management Plan and other plans to ensure that these plans are adapted to work together.
- A vision for the future development of the Knobtown district and a strategy to achieve that vision.

Key Recommendations

LAND USE AND DEVELOPMENT

Guiding Principles

- PROMOTE COMPATIBLE GROWTH
- MAINTAIN THE "RURAL" CHARACTER OF THE AREA
- PROMOTE QUALITY DEVELOPMENT AND IMPROVE AREA APPEARANCE
- PROMOTE SUSTAINABLE DEVELOPMENT
- PROMOTE HOUSING CHOICE
- PROMOTE TRANSPORTATION CHOICE



350 Highway West of Little Blue River



Teetering Rocks Golf Course



Winding Road in Wooded Area

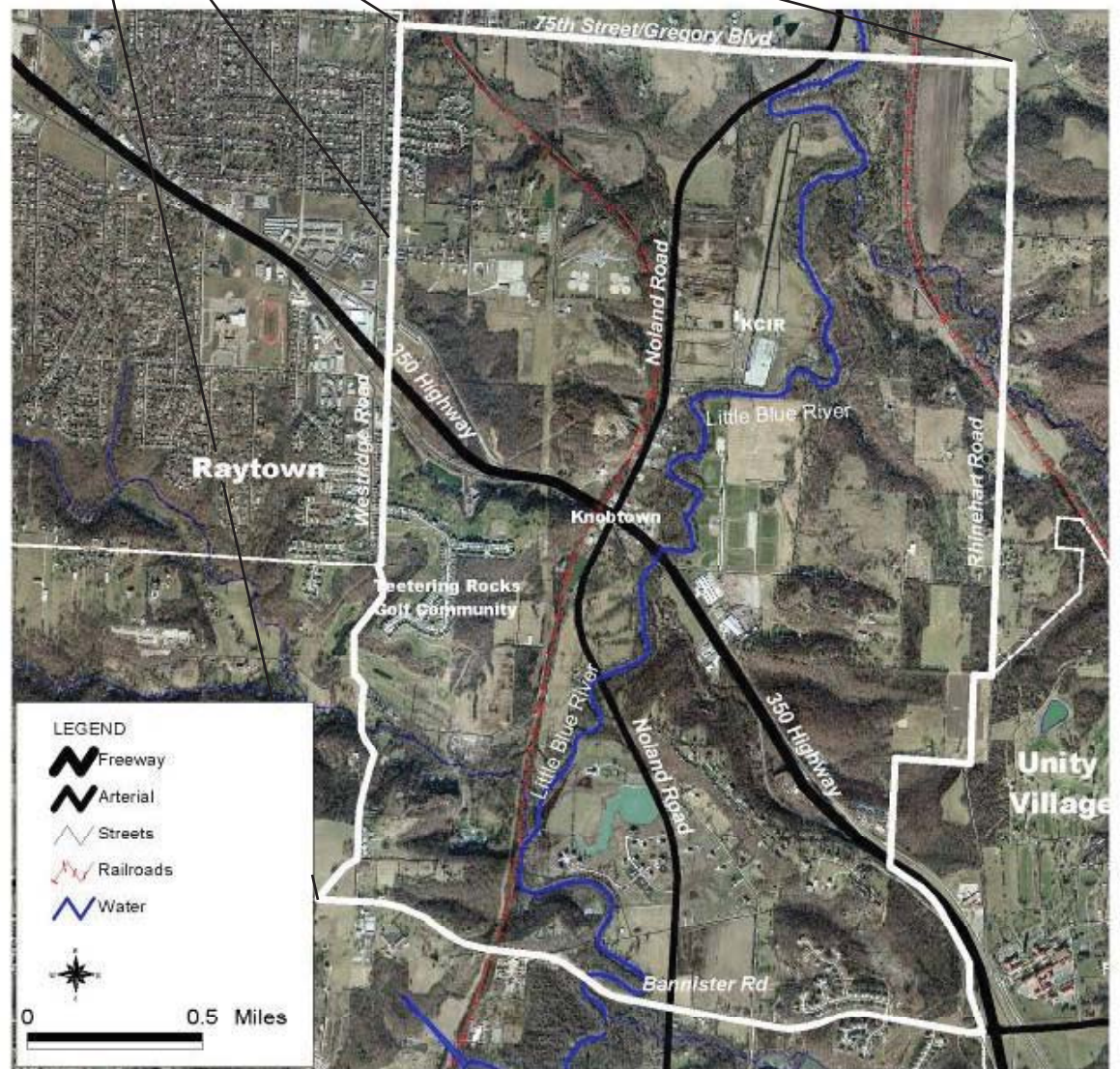


Eastbound 350 Highway at Kansas City City Limits

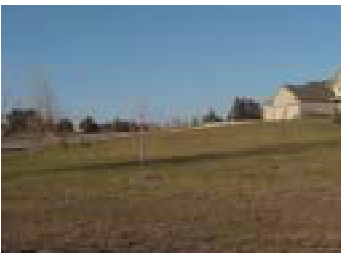
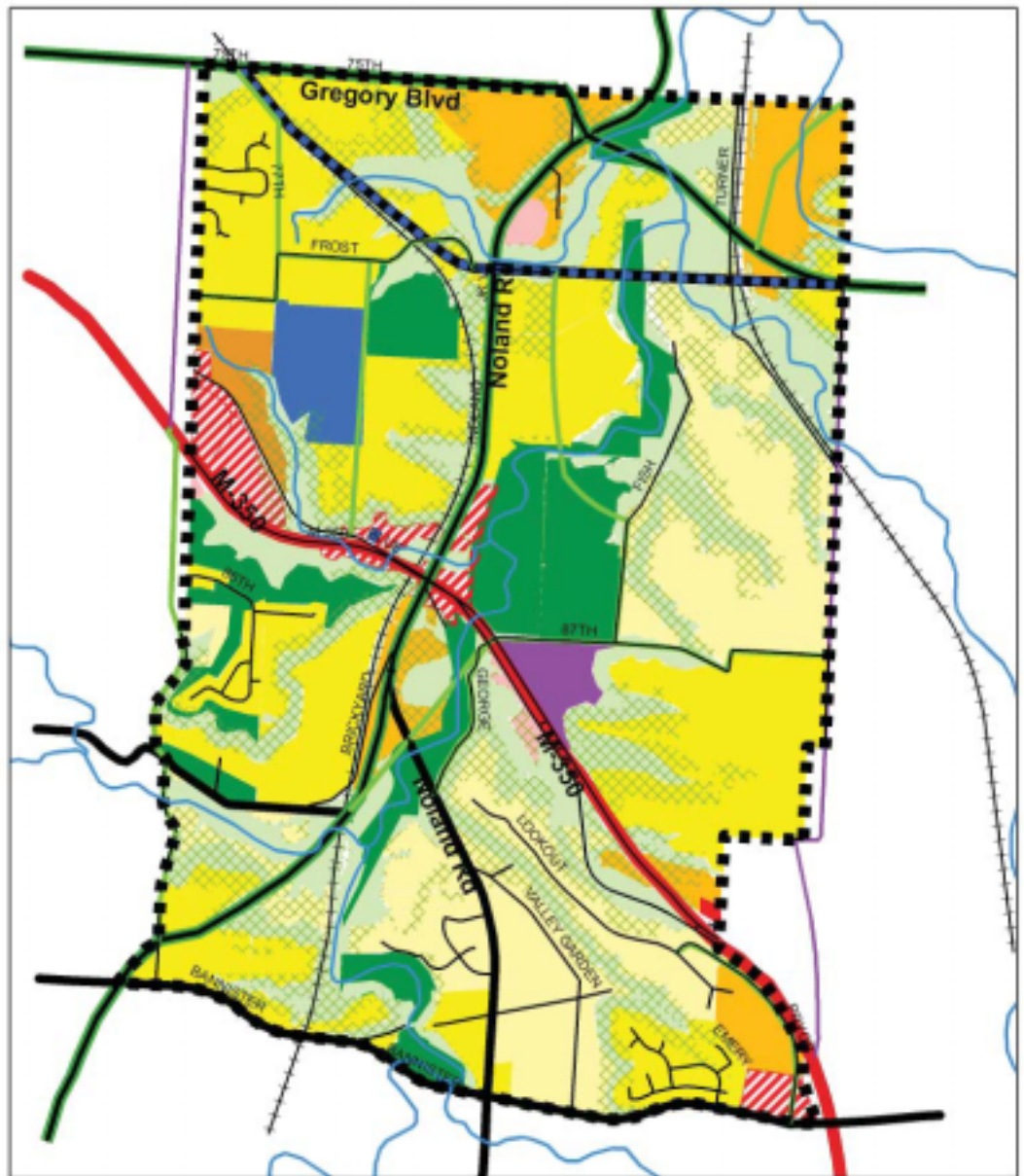


PROJECT BOUNDARIES

- 75th St. to the North
- Bannister Road to the South
- Rhinehart Road to the East
- Westridge Road/Raytown City Limits to the West



Land Use Plan



Residential Very Low



Residential Low



Residential Medium



Residential Medium-High

LAND USE CATEGORIES AND DEFINITIONS

The following land use descriptions serve as a guide for future growth and development within the Plan Area by outlining recommended uses and densities for each category. The land use designations are for planning purposes and do not represent a change to existing uses or zoning.

RESIDENTIAL VERY LOW - Primarily intended for single family detached residential development that allows up to .54 units per acre.

RESIDENTIAL LOW - Primarily intended for single family, detached residential development, but allows a variety of residential building types, and a density up to 5.8 units per acre.

RESIDENTIAL MEDIUM - Intended for a variety of building types that allows up to 8.7 units per acre.

RESIDENTIAL MEDIUM-HIGH - Intended for a variety of building types that allows up to 17.4 units per acre.

MIXED-USE - Primarily intended to accommodate and promote neighborhoods serving retail sales or service uses, as well as mixed-use development consisting of business uses on a building's lower floors and residential uses on upper floors.

COMMERCIAL - Primarily intended to accommodate "heavier" commercial activities and/or operations that are not found in or compatible with mixed-use or neighborhood oriented environments.

OFFICE - Primarily intended to accommodate professional, administrative and corporate office uses (uses that require a large amount of public interface should be reserved for Commercial and Mixed Use areas).

LIGHT INDUSTRIAL - Primarily intended for industrial uses that may include light manufacturing, warehousing, wholesale, storage, distribution centers, office parks, and will allow on-site customers and other less intensive industrial uses.

CONSERVATION DISTRICT - Primarily intended to encourage flexibility in design standards in exchange for 60% open space preservation. These areas allow a variety of residential densities and building types (consistent with the underlying recommended land use).

PARKS - Public or private land reserved for parks and parkways and intended to accommodate active and passive parklands, trails, recreation uses, environmentally sensitive areas, or any other lands reserved for permanent open space purposes.

OPEN SPACE/BUFFER - Consists of private or public lands that are either temporarily or permanently reserved from development, including lands unsuitable for development, stream buffers, floodplains, woodlands, and severe slopes.

DEVELOPMENT GUIDELINES

The physical appearance of development is a major factor in determining public perceptions of an area. Throughout the planning process, area stakeholders expressed a desire for new development to create a sense of place and enhance the area's existing small town/rural character. The plan's Design Guidelines provide a framework for quality development consistent with the Plan Vision.

INFRASTRUCTURE

- Target infrastructure improvements within development priority zones
- Focus public improvements to the "Knobtown" Node at 350 Highway and Noland Road
- New projects will adhere to the KC-ONE Wet Weather Solutions Program Guiding Principles
- Provide infrastructure improvements that are equitable, provide a high quality of life and protect public health and safety
- Opportunity infrastructure improvements: if an infrastructure project is



Mixed-Use Neighborhood



Commercial



Office Development



Light Industrial



Conservation District



Park and Recreation Areas



Open Space



- Target transportation improvements within development priority zones.
- Plan for and build an “economical” road system to meet existing and future needs.
- New roads and roadway improvements should be strategically targeted to areas that are ripe for development.
- Promote a “balanced” transportation system that considers the needs of vehicles, transit, pedestrians, and bicycles.
- Plan for connections to a regional trail system (Little Blue Trace and Katy Trail) in Knobtown and provide connections as development occurs.

PUBLIC SPACES

- IMPROVE THE APPEARANCE OF KNOBTOWN'S STREETS, BRIDGES, AND PUBLIC SPACES - Focus on improving appearance of the Knobtown Mixed-Use node and key "Image" streets.
- REINFORCE AND ENHANCE KNOBTOWN'S RURAL CHARACTER - Preserve existing stone outcroppings and topography throughout the entire area when establishing other aesthetic improvements
- STREETScape AND LANDSCAPE RECOMMENDATIONS - Focus streetscape improvements and enhancements to Primary Image Streets (Noland Road and 350 Highway). Primary Image Streets help set the "tone" of the area by establishing visual and aesthetic standards.
- GATEWAY AND INTERSECTION ENHANCEMENTS - Establish a hierarchy of gateways in the Knobtown area along Primary Image Streets, important intersections and municipal boundaries:
 - » Major Gateways - large scale focal points generally located at the edge of the Plan Area or at the intersections of primary image streets.
 - » Minor Gateways - smaller than Major Gateways and generally highlight particular districts, corridors of distinction and neighborhoods.
- BRIDGE ENHANCEMENTS - Utilize existing bridges on Primary Image Streets for applied enhancements.

IMPLEMENTATION

- THE KNOBTOWN IMPLEMENTATION COMMITTEE - The driving force behind the implementation of the recommendations in the Knobtown Land Use and Development Plan will include business owners, residents, property owners, and any other interested citizens. This group will work with the City and other agencies to oversee implementation of projects recommended in the Knobtown Area.



INTRODUCTION

Purpose and Goals for the Plan

THE KNOBTOWN DEVELOPMENT PLAN PROVIDES:

- A policy framework to strategically guide decisions regarding land use and development, transportation, and public investments.
- A single, coordinated Plan for the area which promotes the citywide initiatives established in the City's Comprehensive Plan (the FOCUS Kansas City Plan) the Major Street Plan, the KC-ONE Stormwater Management Plan and other plans to ensure that these plans are adapted to work together.

GOALS OF THE KNOBTOWN DEVELOPMENT PLAN ARE TO:

- Build on the foundation created by the 350 Highway/Blue Parkway Corridor Plan and the Little Blue Valley Area Plan to establish a specific, implementable strategy for the development of the Knobtown district.
- Create a vision for the future development of the Knobtown district and a strategy to achieve that vision.
- Establish consensus among area stakeholders.
- Promote the creation of a thriving, sustainable and desirable community, through strategies which address:
 - » Land use
 - » Zoning
 - » Infrastructure
 - » Area identity and marketability
 - » Urban design
 - » Transportation
 - » Public safety



350 Highway West of Little Blue River



Teetering Rocks Golf Course



Winding Road in Wooded Area

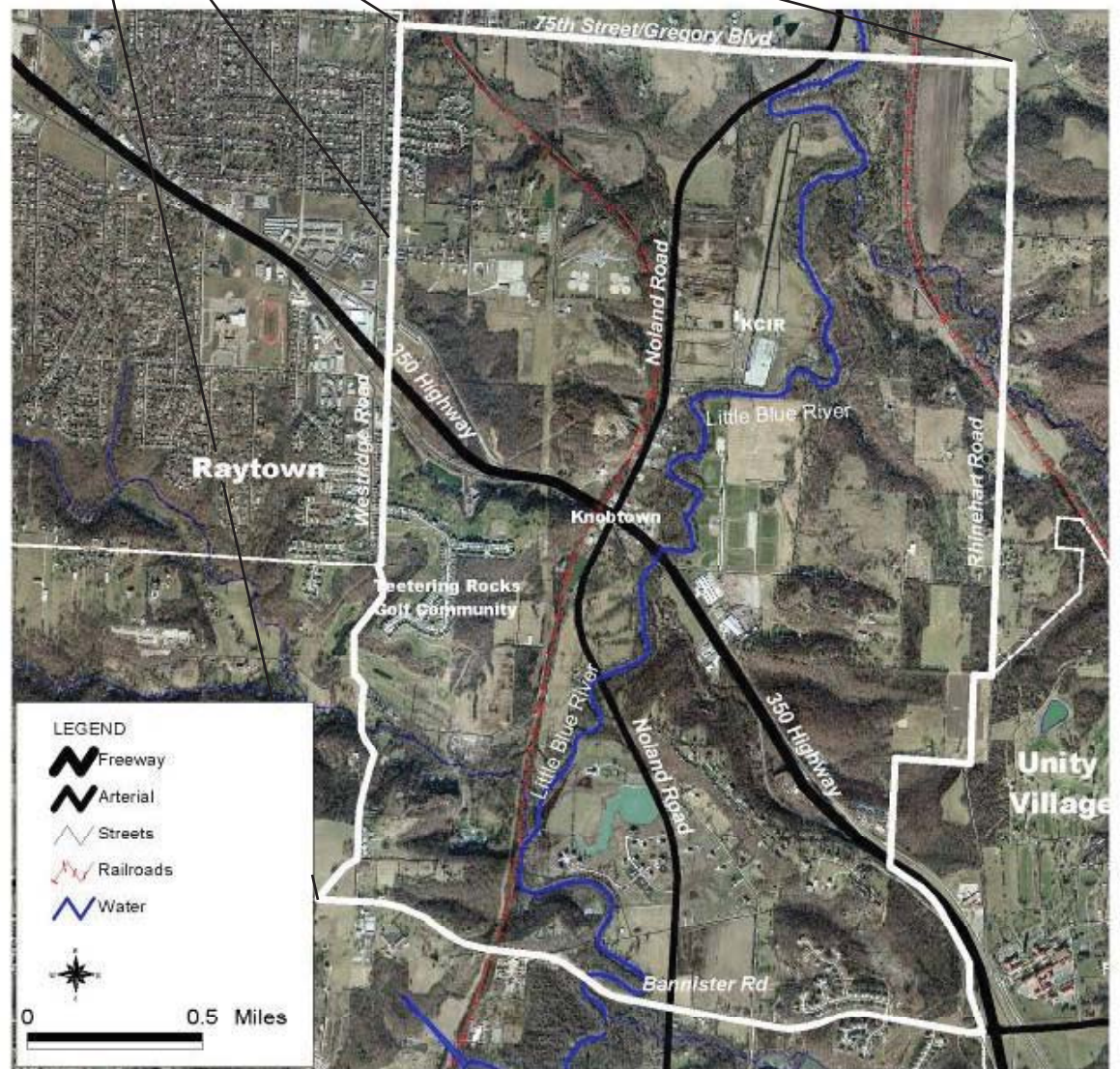


Eastbound 350 Highway at Kansas City City Limits



PROJECT BOUNDARIES

- 75th St. to the North
- Bannister Road to the South
- Rhinehart Road to the East
- Westridge Road/Raytown City Limits to the West



HOW DID KNOBTOWN GET ITS NAME?

How Did Knobtown at Noland Road & 350 Highway Get Its Name? According to local legend, the name of Knobtown, Missouri, comes from an unusual death that occurred there in the late 1800s. A merchant named Charlie Engler had built a general store in about 1897, east of where U. S. 350 now crosses the Little Blue River. A year later, Engler decided to give the community a name and ordered a sign made for "Englersville." A day or two after the sign was prepared, however, Engler's body was found on the basement stairs of the store. His suspenders were wound around his neck and the knob of the basement door. Robbery was a possible motive but the death was ruled a suicide, even though Engler left no suicide note. The community suggested that since Engler had been found hanging from a door knob, Knobtown was a more appropriate name than Englersville. Knobtown was annexed into Kansas City in 1961.

Author: Connie S. Solokoff,
Source: Kansas City Star
Magazine,
Date: July 26, 1987

Knobtown Community Vision Statement

In the future, Knobtown will be a unique community, rich with cultural heritage, natural beauty and rural character in a suburban setting. Through high-quality, sustainable growth, Knobtown will enhance its position as the "Southeastern Gateway" for Kansas City. The Knobtown business district will be thriving. The community will provide a high quality of life for its residents by offering diverse choices for housing, transportation, shopping, services, employment, culture, and recreation in a secure and desirable environment. The infrastructure and transportation systems will be of high quality and will be provided in a strategic and targeted manner. The area's historical values, environmental resources and vistas, and "small town" charm will be preserved and enhanced as new development occurs. Knobtown will be a distinct place that creates a visually pleasing sense of arrival to Kansas City that all Kansas Citians will be proud of.

Background and Orientation

The Knobtown district is generally located in the area around the intersection of Noland Road and 350 Highway where a small commercial node exists. The area is sparsely developed but is located in an area of eastern Kansas City, Missouri (the Little Blue Valley), which has experienced significant growth in recent years (Little Blue Valley population grew by 32% and housing grew by 40% during the 1990's). However, there are some limiting factors for new growth and development in the Knobtown district, including inadequate infrastructure (roads, sewers), topography (steep slopes, floodplains) and sensitive uses (racetrack, adult oriented businesses, and demolition debris landfills).

Knobtown is home to several recreational facilities, including a BMX Track, Raytown Baseball Fields, and Little Blue Trace soccer fields. The main vehicular thoroughfare is 350 Highway which provides convenient and direct access to Kansas City's Central Business Corridor (Country Club Plaza, Westport and Downtown). As a result there is a substantial volume of traffic traveling through Knobtown from high growth areas in southeastern Jackson County. The district serves as Kansas City's southeastern gateway.

Planning Process

To develop a Plan that best represents the concerns and needs of the area's residents, the community was invited to play an active role in the development of the Plan. The planning process utilized extensive public involvement through:

- Close collaboration and coordination with a core project team that included City, County and State staff.
- A Steering Committee appointed by the Mayor comprised of area stakeholders.

- Public “town hall” type meetings to gather ideas, information, and concerns; identify strengths, weaknesses, opportunities, and threats; present alternative concepts and collect feedback on those alternative concepts; and to present final recommendations for establishing consensus and reinforcing civic ownership of the Plan.

Planning Issues

The following is a list of general issues identified through the planning process (public input and technical analysis) that are addressed in the plan:

- Provide a predictable and orderly pattern of development.
- Improve the image and appearance of the area through design standards for new development and enhancements of public spaces.
- Provide improved shopping and services for the area.
- More diverse housing choices in the area.
- Protect and enhance the area’s historic and cultural resources.
- Preserve environmental resources as the area develops and preserve the natural beauty of the area.
- Strategically improve existing roads and provide safe and adequate circulation (for both existing development and future development).
- Improve multi-modal transportation options in the area (bike, pedestrian, transit and trails).
- Provide adequate infrastructure to serve existing development and guide new development with strategic improvements/ extensions.
- Address issues related to the rural character and relative isolation of the area (illegal dumping, service delivery, unwanted land uses).
- Address existing “sensitive” land uses which may potentially discourage new development.
- Plan for the orderly and phased development of the area and ensure that new development is compatible and of high quality.
- Enhance existing strong neighborhood organization and involvement and continue to increase involvement.



The Battle of White Oak Creek site and the stone house on Noland Road are both historic features that should be preserved in the future. These resources will contribute to the character of the area.

LAND USE and development

Introduction

The following chapter provides a guide for future development within the Plan Area that is based on economic reality and a balance between future development and the protection of neighborhoods, the natural environment, infrastructure considerations, and the area's existing character.

Key Issues

The following issues were identified during the planning process:

- Discourage new “sensitive” and potential nuisance uses such as landfills, quarries and adult oriented uses.
- Preserve the low-density character of the area and provide appropriate locations for higher density housing.
- Preserve the rural character, historic assets and environmental resources in the area.
- Improve the appearance of commercial areas.
- Attract additional businesses and professional services that meet neighborhood needs.
- Provide predictability and certainty about the compatibility of future development.
- Ensure that development occurs in an orderly and efficient way and that adequate roads and infrastructure are in place before development is permitted.

Land Use Guiding Principles

PROMOTE COMPATIBLE GROWTH

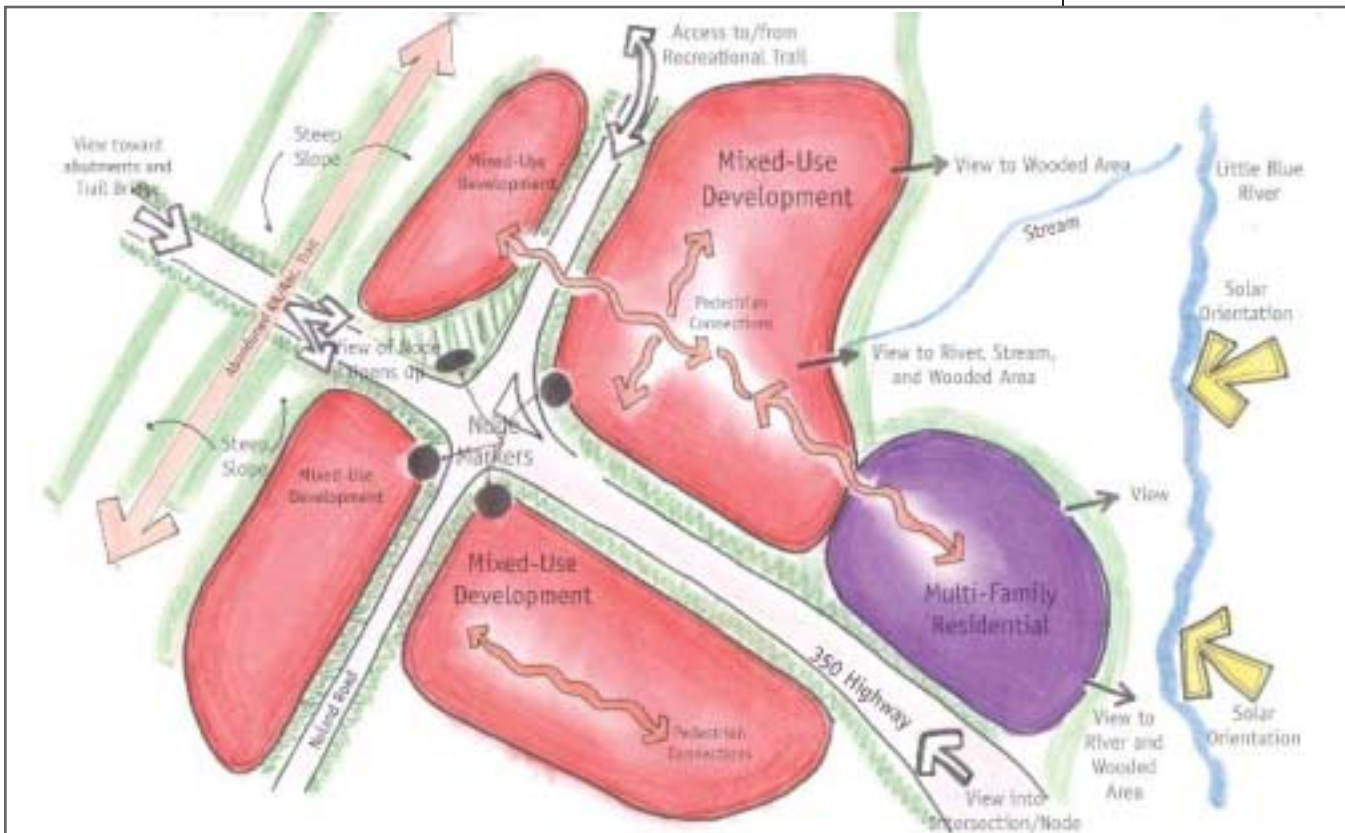
- New development should be compatible to adjacent properties in terms of intensity, design, density, massing and scale.
- Low Density Residential and Open Space uses should be the predominant uses in the area and other more intense uses should be located along major corridors.
- New development or expansions of existing uses that cause excessive noise or other nuisances such as adult oriented uses should be discouraged. New industrial zoning or conditional use permits for mining, quarrying, rock crushing, land fills or racetracks (or similar uses) are generally not appropriate.
- Where higher intensity uses abut lower intensity uses a transition in height and density should be provided (see Transition Guidelines).
- Infill development and redevelopment should be well integrated with existing development areas and the natural environment.
- New development should comply with the MODOT access management guidelines.

SENSITIVE USES that are not desirable from a special interest point-of-view, should be appropriately screened and designed to be compatible with the overall neighborhood or community character. Such uses also should not be concentrated in a particular area and should demonstrate some community benefit.

FOCUS Kansas City Plan

PROMOTE “NODE” DEVELOPMENT

- Commercial or mixed-use development should be limited to nodes at major intersections as recommended in the 350 Highway/Blue Parkway Corridor Plan (see Figure 2: Land Use Plan):
 - » Westridge Road
 - » Knobtown Intersection of 350 Highway and Noland Road (see below)
 - » Bannister Road/Colbern Road
- Strip commercial development should be strongly discouraged.



NOBTTOWN NODE DEVELOPMENT RECOMMENDATIONS

- Public Spaces (see page 31)
 - » Provide “Focal Points” at intersections
 - » Implement Streetscape recommendations
- Building Design and Orientation (see Development Guidelines on page 39)
 - » Orient buildings toward roads with parking on the side or behind
 - » Encourage compact building development to promote walkability and shared parking between businesses
 - » Design buildings to present a “front door” impression to the street
 - » Use buildings materials that compliment the Knobtown character and create a sense of cohesive district design
- Water Quality
 - » Stream and River buffers
 - » Encourage stormwater BMP’s such as Bioswales and pervious paving
- Walkability
 - » Clearly define pedestrian paths
 - » Support pedestrian circulation throughout and between sites
 - » Create access to recreational trails throughout the community
- See Development Guidelines chapter for more design guideline recommendations.



The dense woods and agricultural uses of the Knobtown area should be maintained and should be contributors to the character of future development.



Primary intersections should be improved to contribute to the identity of the area.

DEVELOPMENT PRIORITY ZONES are identified for promoting quality and contiguous development in Kansas City. These zones are identified in the FOCUS Kansas City Plan.

- Recommends the City proactively guide development in these areas.
- Development outside a Development Priority Zone should only be encouraged when it meets the larger FOCUS Kansas City Plan objective.
- Developers should bear the full cost of infrastructure improvements in areas not identified as a Development Priority Zone.

MAINTAIN THE “RURAL” CHARACTER OF THE AREA

- Integrate natural features, streams, topographic features, scenic views and trees in Development Plans.
- Implement a system of connected open space and trails.
- Encourage the protection of viewsheds within the Knobtown area.

PROMOTE QUALITY DEVELOPMENT AND IMPROVE AREA APPEARANCE

- New development should incorporate a high level of architectural detail, innovative design and high quality materials (see Development Guidelines).
- Public investments to improve the appearance of public streets and spaces should be provided to lead private investment and encourage quality development.
- Enhance the appearance of 350 Highway as a primary “image” street and eastern gateway into the City. Efforts should be focused on streetscape and gateway features at the Noland Road and Westridge Road intersections.
- As development occurs along major streets and image streets (particularly in the “median” areas on 350 Highway) ensure high quality, complementary, and visually pleasing buildings, materials, landscaping, and signage (see Development Guidelines).
- Explore ways to provide incentives for building and site improvements along primary image streets (Noland Road, 350 Highway).

PROMOTE SUSTAINABLE DEVELOPMENT

- New development should be targeted within “Development Priority Zones” or areas that are contiguous to existing development that utilize existing infrastructure. (See Figure 1: Development Priority Zones)
- New development should provide adequate infrastructure, including streets, sewers, and water.
- Public improvements and investment should be targeted to lead private investment in “Development Priority Zones,” and should be based on Citywide priorities.
- Preserve and enhance historic and cultural resources as development occurs.
- Focus the land use structure to be integrated within the surrounding environmental characteristics of the area to the greatest extent possible including the Little Blue River as an important element of the Knobtown community environment.
- Emphasize stormwater management approaches which enhance environmental stewardship and natural resource preservation according to the goals and objectives of the City’s Wet Weather Solutions Program.
- Sustainable materials should be encouraged within new development.

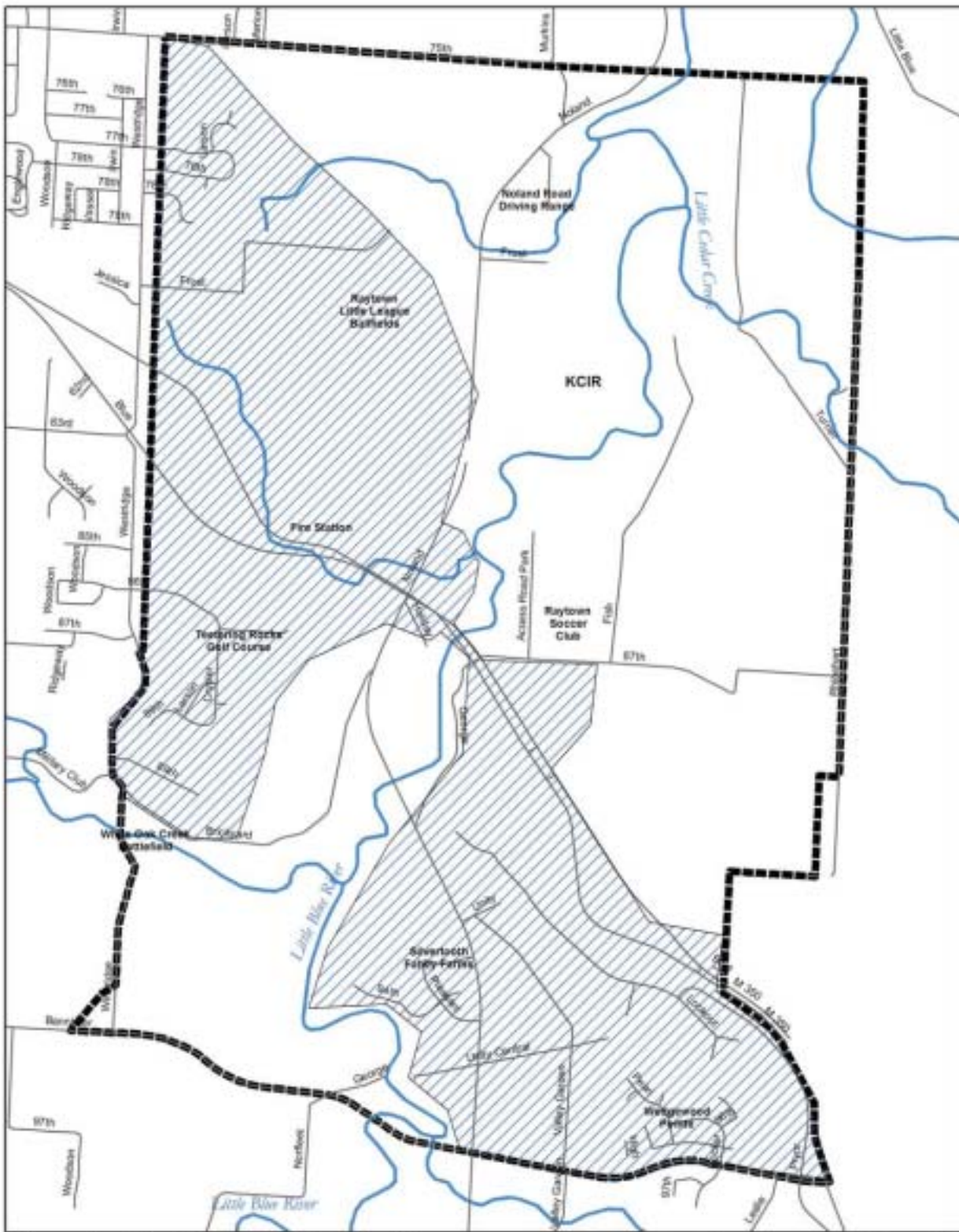


Figure 1: Development Priority Zones

- Development Priority Zones
- Roads
- Streams

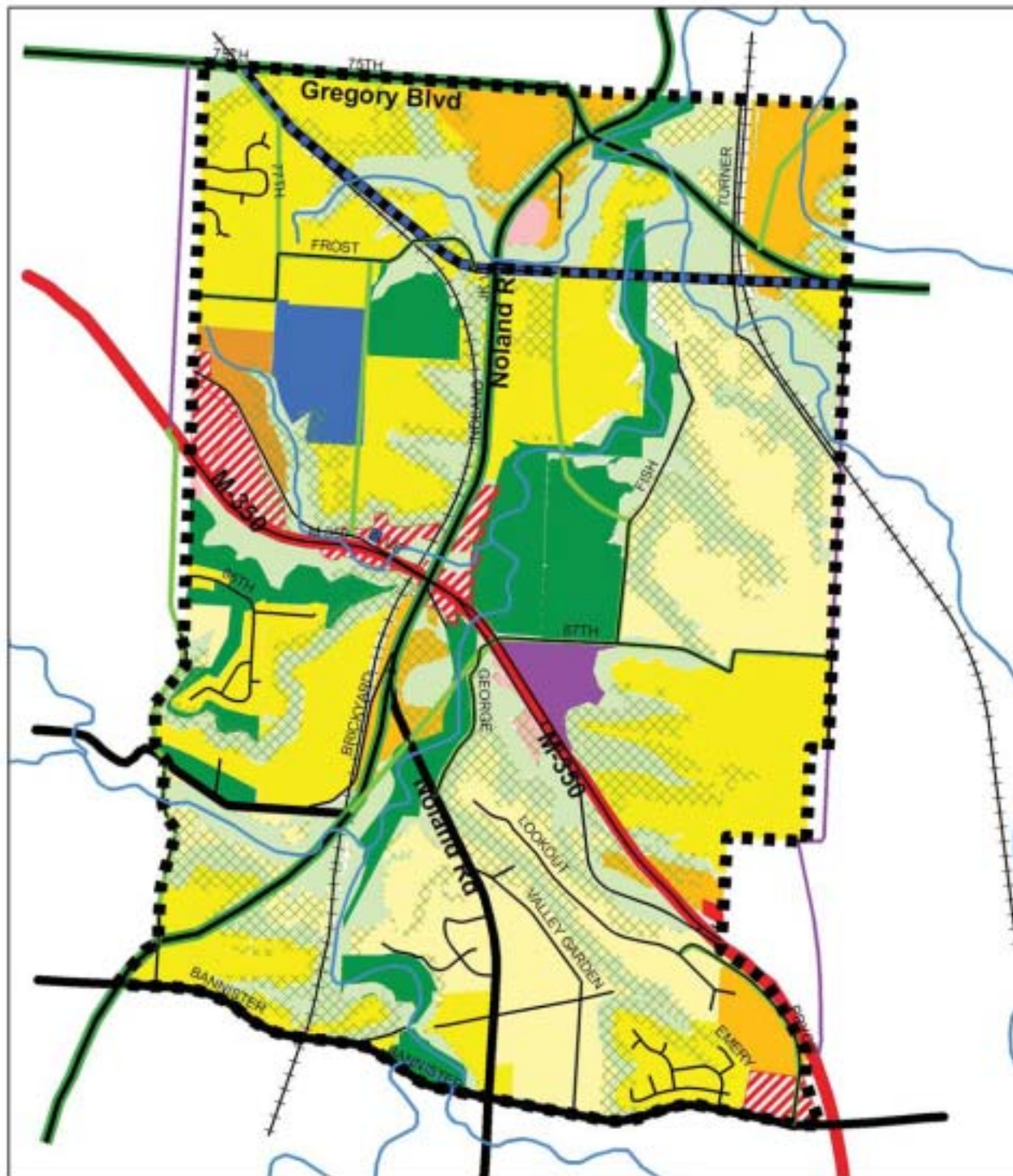


Figure 2: Land Use Plan

Land Use Plan

- | | | |
|---------------------------------|------------------------|--------------------------------------|
| Residential Very Low Density | Mixed-Use Neighborhood | Freeway/Interstate |
| Residential Low Density | Light Industrial | Parkway/Boulevard |
| Residential Medium Density | Park | Primary Arterials |
| Residential Medium-High Density | Open Space | Secondary Arterial |
| Institutional | Office | Other Knobtown Area Roads |
| Office | Conservation Overlay | Alternate Alignment for Gregory Blvd |
| Knobtown Study Area Boundary | Roads | Proposed Collector |



PROMOTE HOUSING CHOICE

- Allow a diverse range of housing types and densities while preserving areas for traditional low density development.
- Prohibit multi-family development within established single-family residential areas. Medium Density Residential land use should be used as a transition between low density residential uses and more intense uses.
- Medium Density Residential should be located along major thoroughfares and near mixed use and employment areas.

PROMOTE TRANSPORTATION CHOICE

- Provide for pedestrian connections between new development and adjacent developments, trails, parks and other open spaces and transit stops.
- Accommodate transit riders and bicyclists through site design and amenities.
- Construct a system of collector and arterial streets as development occurs (see Transportation Chapter). Implement a system of connected streets and neighborhoods as the area continues to develop.

Land Use Plan

The following land use descriptions serve as a guide for future growth and development within the Plan Area by outlining recommended uses and densities for each category (see Figure 2: Land Use Plan). The land use designations are for planning purposes and do not represent a change to existing zoning.

RESIDENTIAL VERY LOW

Primarily intended for single family detached residential development that allows up to .54 units per acre. This land use classification will correspond to the R80 zoning category in the new zoning ordinance.



Residential Very Low

RESIDENTIAL LOW

Primarily intended for single family, detached residential development, but allows a variety of residential building types, and a density up to 5.8 units per acre. This land use classification will correspond to the R7.5 and R10 zoning categories within the new zoning ordinance.



Residential Low

RESIDENTIAL MEDIUM

Intended for a variety of building types that allows up to 8.7 units per acre. This land use classification will correspond to the R5, R6, and R7.5 zoning categories within the new zoning ordinance.



Residential Medium



Residential Medium-High



Mixed-Use Neighborhood



Commercial



Office Development



Light Industrial

RESIDENTIAL MEDIUM-HIGH

Includes small lot single-family development, town homes, duplexes and apartments up to 17.4 units per acre. This land use classification will correspond to the R2.5 zoning category within the new zoning ordinance.

MIXED-USE NEIGHBORHOOD

Primarily intended to accommodate and promote neighborhoods serving retail sales or service uses, as well as mixed-use development consisting of business uses on a building's lower floors and residential uses on upper floors. This land use classification will correspond with the B1 and B2 zoning category within the new zoning ordinance.

COMMERCIAL

Primarily intended to accommodate "heavier" commercial activities and/or operations that are not found in or compatible with mixed-use or neighborhood oriented environments. This land use classification will correspond with the C zoning category within the new zoning ordinance. Includes large-scale commercial development targeted in designated areas along major arterials with highway access.

OFFICE

Primarily intended to accommodate professional, administrative and corporate office uses (uses that require a large amount of public interface should be reserved for Commercial and Mixed Use areas). This land use classification will correspond with the O1 and O2 zoning categories within the new zoning ordinance.

LIGHT INDUSTRIAL

Primarily intended for industrial uses that may include light manufacturing, warehousing, wholesale, storage, distribution centers, office parks, and will allow on-site customers and other less intensive industrial uses. These areas are intended to promote the economic viability of industrial uses; encourage employment growth; and limit the encroachment of unplanned residential and other non-industrial development into industrial areas. This land use classification will correspond with the M1 zoning category within the new zoning ordinance. Discourage uses that include new sensitive uses such as quarrying, adult businesses, landfills and industrial uses with potentially noxious impacts.

CONSERVATION DISTRICT

Primarily intended to encourage flexibility in design standards in exchange for 60% open space preservation. These areas allow a variety of residential densities and building types (consistent with the underlying recommended land use). These areas will provide additional open space and recreation amenities for residents, preserve environmentally sensitive resources, and will reduce storm water runoff and water pollutants. This land use classification will correspond with the Conservation Development option within the new zoning ordinance.



Conservation District

PARKS

Public or private land reserved for parks and parkways and intended to accommodate active and passive parklands, trails, recreation uses, environmentally sensitive areas, or any other lands reserved for permanent open space purposes.



Park and Recreation Areas

OPEN SPACE/BUFFER

Consists of private or public lands that are either temporarily or permanently reserved from development, including lands unsuitable for development, stream buffers, floodplains, woodlands, and severe slopes.



Open Space

INFRASTRUCTURE

Introduction

Infrastructure needs in Knobtown are significant and resources to address those needs are limited. The provision of adequate infrastructure in the Knobtown area is important to serve the needs of the community, encourage new development, and protect the public health and environmental quality of the area. Infrastructure improvements must be carefully prioritized and targeted.

Key Issues

SANITARY SEWER

- There are large areas in Knobtown with no access to sanitary sewers, or that possess sewers that are undersized. A strategy and guidelines to establish sanitary sewer service to areas not currently served is needed.
- Construction costs of extending sanitary sewer into the Knobtown area should not be solely the responsibility of the City. Sewer expansion costs to serve this area should be determined by a sewer study.

SANITARY SEWER COVERAGE	TOTAL ACRES	PERCENTAGE
No Coverage	1061.4	39.7
Onsite Wastewater	557.3	20.8
Sanitary Sewer	946.1	35.4
Both	108.9	4.1
Total	2,673.7	100.00

WATER

- Portions of the Plan Area have inadequate water mains (smaller than six inches in diameter, do not support fire protection, do not support residential development) or are without water service.
- Although the area south of 350 Highway appears to have an acceptable distribution network for the existing users, larger diameter mains may be necessary to serve future developments in the area.
- The existing mains north of 350 Highway appear to marginally serve the existing users.

Source: JEO Consulting Group, INC.
based from Gregory and Rhinehart
Road Septic Tank Study Area Map

-  Septic Tanks
  Inactive Sewer Main
  Sanitary Sewer
  Knobtown Study Area Boundary
  Roads
- Wastewater Treatment Type**
 On Site
 Sanitary
 Both
 None

STORMWATER MANAGEMENT

- There are significant stormwater management needs in the area. The Little Blue River Watershed Master Plan identifies existing deficiencies in the stormwater management system in the area and recommends improvement, such as stream stabilization, stream buffers, preservation of wetlands, bridge/culvert improvements, and stormwater system improvements.

Guiding Principles

TARGET INFRASTRUCTURE IMPROVEMENTS WITHIN DEVELOPMENT PRIORITY ZONES:

- Prioritize infrastructure projects within existing developed areas.
- Promote strategic and efficient extensions of existing infrastructure to facilitate development in contiguous areas (See Figure 4: Development Priority Zones/Wastewater Coverage).
- Provide infrastructure in a strategic and targeted manner that supports future land development opportunities.
- Public improvements and investment should be targeted to spur private investment in “Development Priority Zones”. Any investments must be weighed against other citywide priorities.

FOCUS PUBLIC IMPROVEMENTS TO THE “KNOBTOWN” NODE AT 350 HIGHWAY AND NOLAND ROAD:

- Stormwater management, sanitary sewer and roadway (see Transportation Chapter) improvements are needed in this area to facilitate future development of this mixed-use node.
- The aesthetic and transportation recommendations in the 350 Corridor Plan should also be prioritized in this area (see Transportation Chapter and Public Spaces Chapter).

NEW PROJECTS WILL ADHERE TO THE KC-ONE WET WEATHER SOLUTIONS PROGRAM GUIDING PRINCIPLES:

- Through strong creative leadership and a stewardship ethic, the Wet Weather Solutions Program will take action to manage the City’s water resources in a sustainable way.
- Watershed-based: Consider all sources of problems and solutions so that strategies account for the interrelationship of water, land use, air quality, and human communities within a watershed, leading to project outcomes with multiple benefits.
- Maximize environmental, community and economic benefits: Preference given to options that create multiple benefits for the community, environment, and the regional economy so that the legacy is a stronger, more appealing, and more prosperous community.

0 0.125 0.25 0.5 Miles

 On Site  Septic Tanks
 Sanitary  Sanitary Sewer Line
 Both  Inactive Sewer Line
 None  Development Priority Zones

- Financial: Manage the community's resources with a long-term view, pursuing fairness in the distribution of the economic benefits and burdens.

PROVIDE INFRASTRUCTURE IMPROVEMENTS THAT ARE EQUITABLE, PROVIDE A HIGH QUALITY OF LIFE AND PROTECT PUBLIC HEALTH AND SAFETY:

- Ensure all area residents and businesses have cost efficient and reasonable access to public water and wastewater services, and provide adequate storm water improvements while providing mechanisms for funding that represent equitable cost-sharing.
- Continuously evaluate and improve utility systems within existing neighborhoods as well as providing extensions for new continuous development.

OPPORTUNITY INFRASTRUCTURE IMPROVEMENTS:

- If an infrastructure project is funded and let within the Plan Area, the applicable City department or agency will reference this Plan and coordinate identified improvements with other departments and agencies. For example, if an undersized water line is upgraded, other improvements such as the installation of a sidewalk or trail should be completed concurrently where possible.

PRIORITIZED IMPROVEMENTS

- While it is not practical or necessary to construct water and/or sewer services to cover every area of the Knobtown area, it is recommended that improvements be prioritized to areas contiguous to existing development and established in an orderly method. Improvements should be generally designed and constructed to meet both existing and future development needs.

PRIORITIZED INFRASTRUCTURE IMPROVEMENTS

PLAN



The following recommended priority framework for infrastructure improvements are intended to provide a strategic approach to address the areas significant needs and limited resources. This strategy represents a general order for public investments. However, future opportunities may arise that don't follow this phasing but meet the decision making criteria and intent of this Plan. (see Implementation Chapter)

For a listing of recommended infrastructure improvements, see the Implementation Chapter. Infrastructure improvement areas are graphically displayed in Figure 5: Prioritized Areas of Infrastructure Improvements.

PRIORITY AREA 1 (DEVELOPMENT PRIORITY ZONE NORTHWEST):

These improvements are intended to serve existing developed areas as well as encourage new development within the development priority zone. Extend interceptor sanitary sewers to basins on the western portion of the Development Priority Zone (based upon Citywide priorities). Investments should be generally focused to areas north of 350 Highway and to serve the "Knobtown" mixed use node (intersection of Noland Road and 350 Highway). Upgrade undersized waterlines in these areas as well and implement stormwater improvements as recommended in KC-ONE.

PRIORITY AREA 2 (DEVELOPMENT PRIORITY ZONE SOUTHEAST):

Sewer expansion should be based, to an extent, on the desires of the neighborhood. Sewer improvements should be based on an area-wide sewer study. Upgrades to undersized water mains and stormwater improvements should be implemented in this area as well.

PRIORITY AREA 3 (AREAS CONTIGUOUS TO DEVELOPMENT PRIORITY ZONE):

Throughout the process, participants identified these areas as a long-term priority. Infrastructure extensions and improvements in these areas should generally be provided by private development and property owners.

OTHER "GREENFIELD" AREAS:

These areas are not seen as a priority for the expansion of infrastructure. Possible exceptions might be considered if an area is adjacent to existing development served by infrastructure. Improvements in these areas should be limited to maintenance and replacement of existing systems without extension of infrastructure to support or encourage new urban development

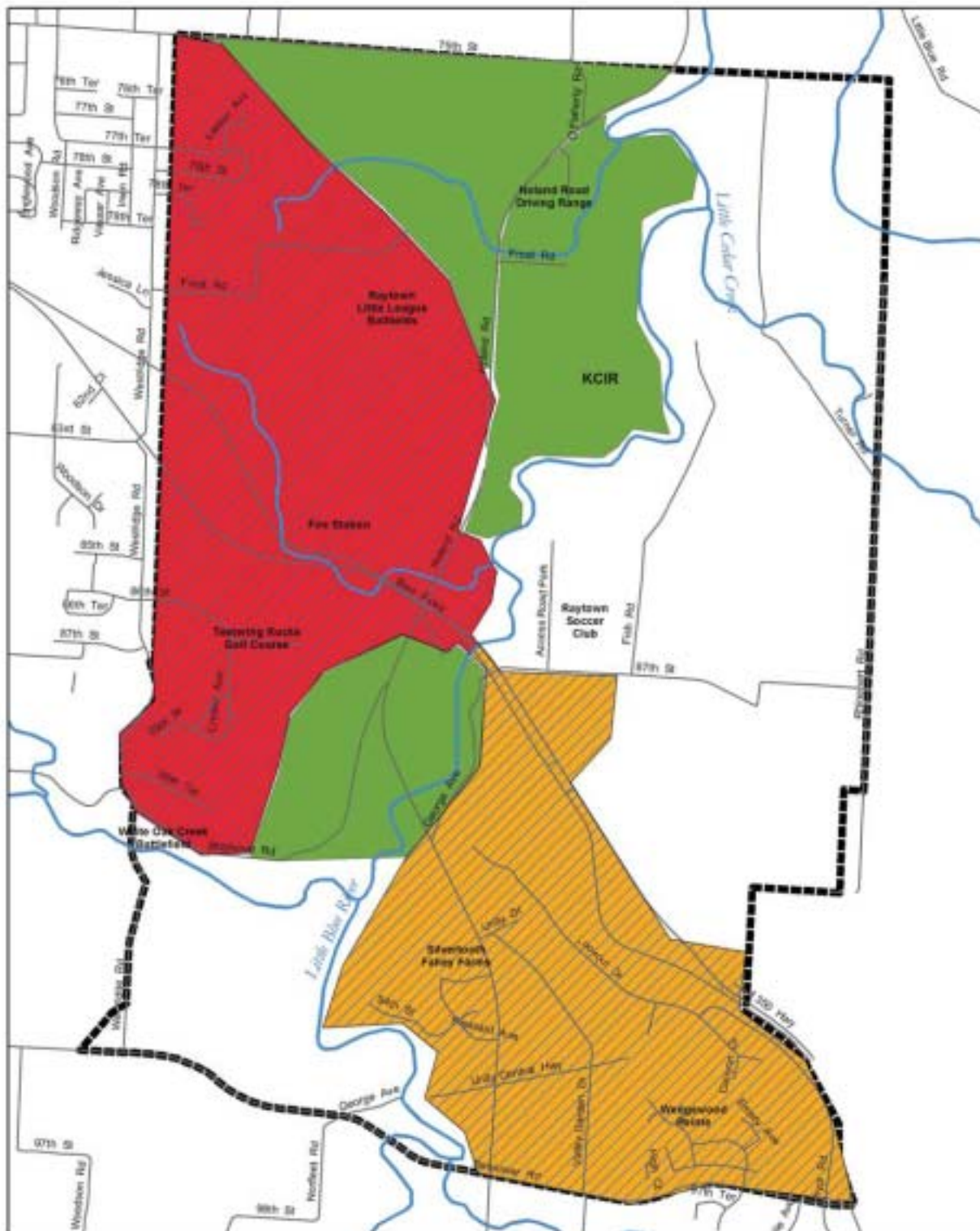


Figure 5: Prioritized Areas of Infrastructure Improvements



STORMWATER RECOMMENDATIONS

IMPLEMENT THE **KC-ONE STORMWATER MANAGEMENT PLAN**

The **KC-ONE Stormwater Management Plan** will detail the city's strategy, policy and administrative plans for the future of the stormwater management program. The mission of KC-ONE is to create "one Plan, one People, and one Voice for the management of stormwater throughout Kansas City" through an analysis of key environmental conditions including steep slopes, vegetative cover, depressional areas, floodplains, wetlands and stream health, KC-ONE identified two Stormwater Management Framework Plans:

- Stream Buffer Framework (See Figure 6: Stream Buffers): Identifies environmentally sensitive areas within and adjacent to all streams within the Plan Area that are to be protected and preserved.
- Opportunity Areas for Innovative Stormwater Management Techniques: Identifies opportunity areas for the integration of innovative stormwater management solutions based upon best management practices, also known as "Stormwater BMPs." the results of these analyses and recommendations are included on the following pages.



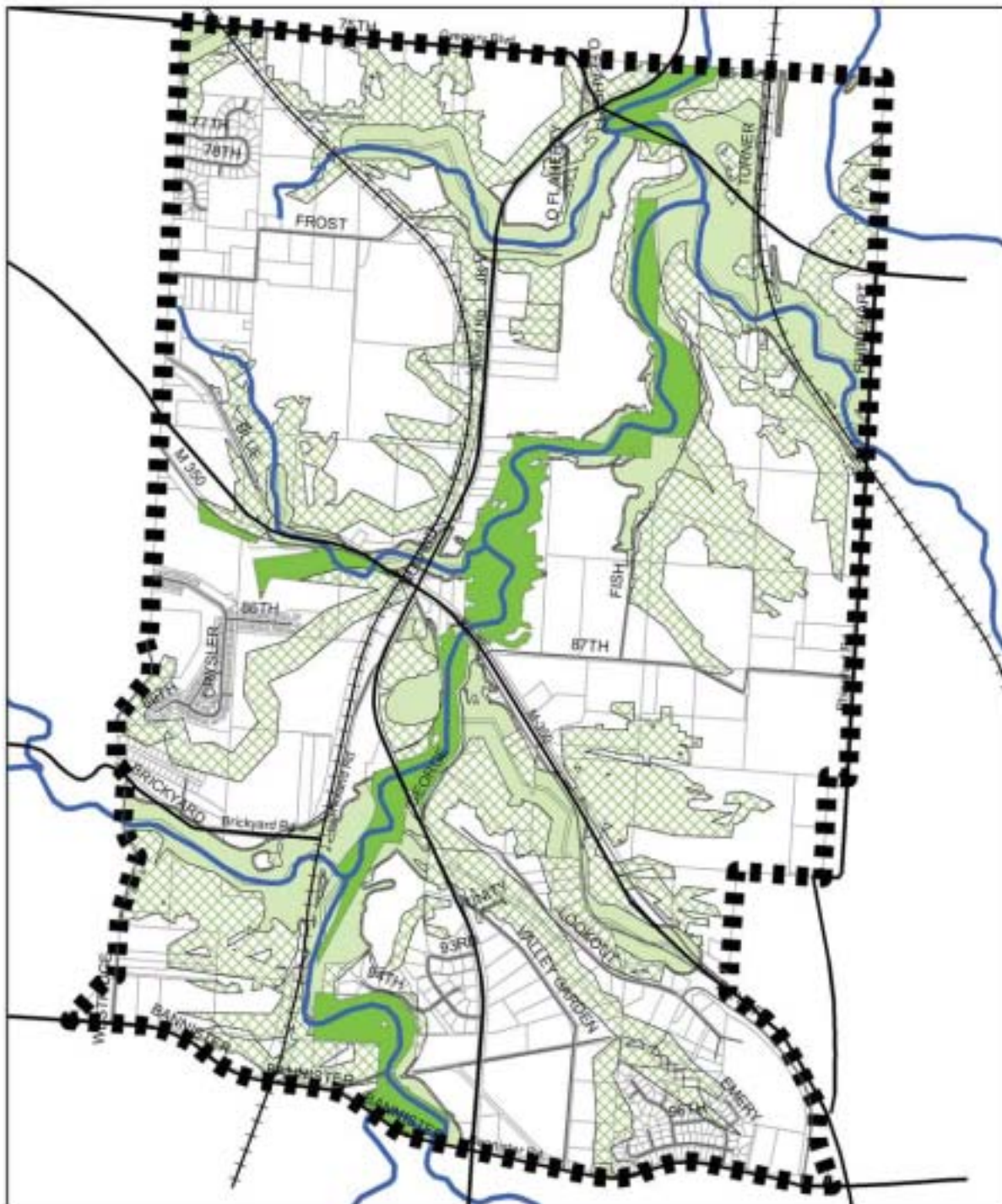
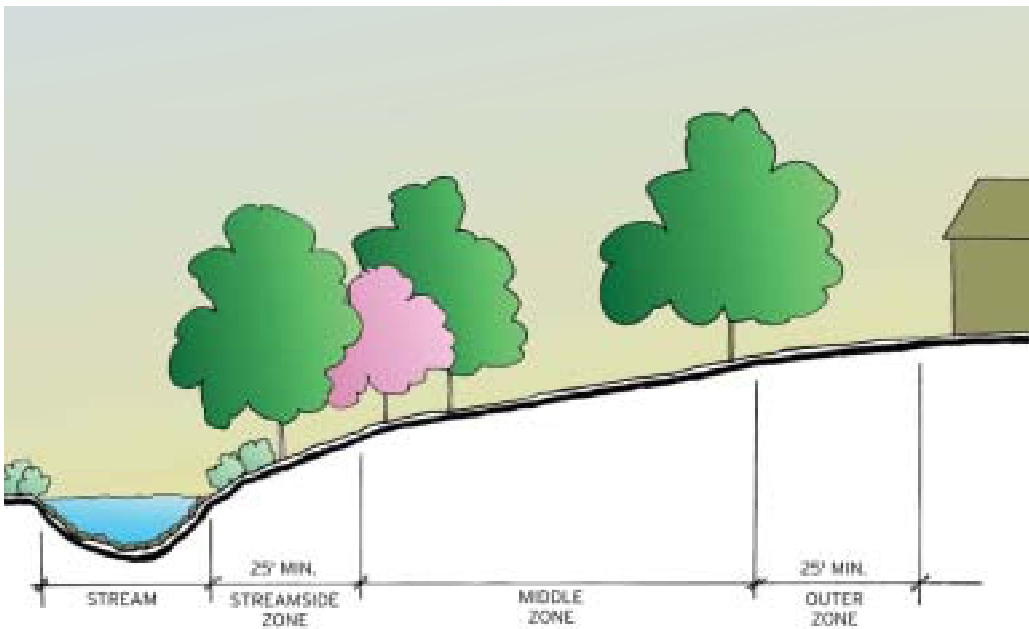


Figure 6: Stream Buffers

- | | |
|---------------|------------------------|
| — Major Roads | □ Parcels |
| — Roads | ▨ Conservation Overlay |
| + Railroads | ▩ Open Space |
| — Streams | ■ Park |





Cross-section of stream set-back areas

STREAM BUFFER GUIDELINES

All development proposals, projects, and public improvements within identified buffer areas will adhere to these recommendations.

STREAMSIDE AREAS

Description: Area bounded by a 25 foot strip adjacent to the top of the stream or creek bank.

Uses: These areas should be reserved for green space preservation and restoration of native habitats. All other development is restricted.

MIDDLE AREAS:

Description: Variable zone that include the 100-year floodplain and/or national wetlands. Minimum distances from top of bank (from 0 to 250-feet) would apply if there is no mapped floodplain.

Uses: Some limited passive recreation uses and utilities may occur within these areas such as unpaved pedestrian and bike trails and natural stormwater management practices.

OUTER AREAS:

Description: Variable zone extending a minimum of 25-feet from the middle zone. These areas include slopes greater than 15%, or likely natural habitat areas.

Uses: Conservation Residential developments are recommended within and adjacent to these areas to preserve the outer, middle and streamside areas with a minimum of 60 percent open space. (See Land Use and Development Chapter, for land use guidelines within these areas.)

OPPORTUNITY AREAS FOR INNOVATIVE STORMWATER MANAGEMENT TECHNIQUES

As part of KC-ONE, areas have been identified for the integration of stormwater best management practices (BMP's). All development proposals, projects and public improvements within these areas should consider the following stormwater management techniques or BMPs. More detailed standards and specifications for all of listed BMPs are referenced in the **Manual of Best Management Practices for Storm Water Quality** or "BMP Manual", adopted in October 2006. It should be noted that BMPs are most effective when applied in a "treatment train," meaning a series of treatments combined to improve the area's overall water quality.

TRANSPORTATION



Introduction

A majority of the transportation network in the Plan Area was developed well before the area was annexed into Kansas City and most roads in the area are built to a rural standard. Some roads have been improved as development has occurred, but the area remains largely undeveloped and most roads are inadequate to support urban development. Furthermore, few accommodations have been made for pedestrians or bicyclists. This chapter provides a guide for strategic and phased transportation improvements within the Plan Area.

Key Issues

The following issues were identified by Work Team participants, and analysis of existing conditions:

IMPROVE CAPACITY AND SAFETY OF EXISTING ROADS

- Major roads in the area are not improved to City standards and are typically unable to handle the capacity they are intended to carry (according to Major Street Plan designation). 350 Highway is the only Major Street (as designated on the Major Street Plan) that is improved to major street standards.
- Many roads are very narrow, have dangerous curves, and insufficient sight lines.
- Several narrow bridge underpasses remain from the former Union Pacific railroad corridor. These underpasses severely inhibit access and circulation, and constrain development.

STRATEGICALLY COMPLETE GAPS IN THE MAJOR STREET PLAN

- There are two roads on the Major Street Plan that do not exist (Gregory Boulevard and East Longview Parkway). The detailed future alignment and phasing of construction for these roads needs to be established.
- A system of collector streets needs to be implemented in the area to connect neighborhoods to arterial streets and reduce future traffic on arterial streets. These streets should be constructed prior to development or as development occurs.

ACCOMMODATE ALL TRANSPORTATION MODES

- Throughout the process, participants noted the need to plan for all transportation modes including vehicles, transit, pedestrians, and bicycles.



New development should accommodate multiple modes of transportation, including pedestrians and bicycles.

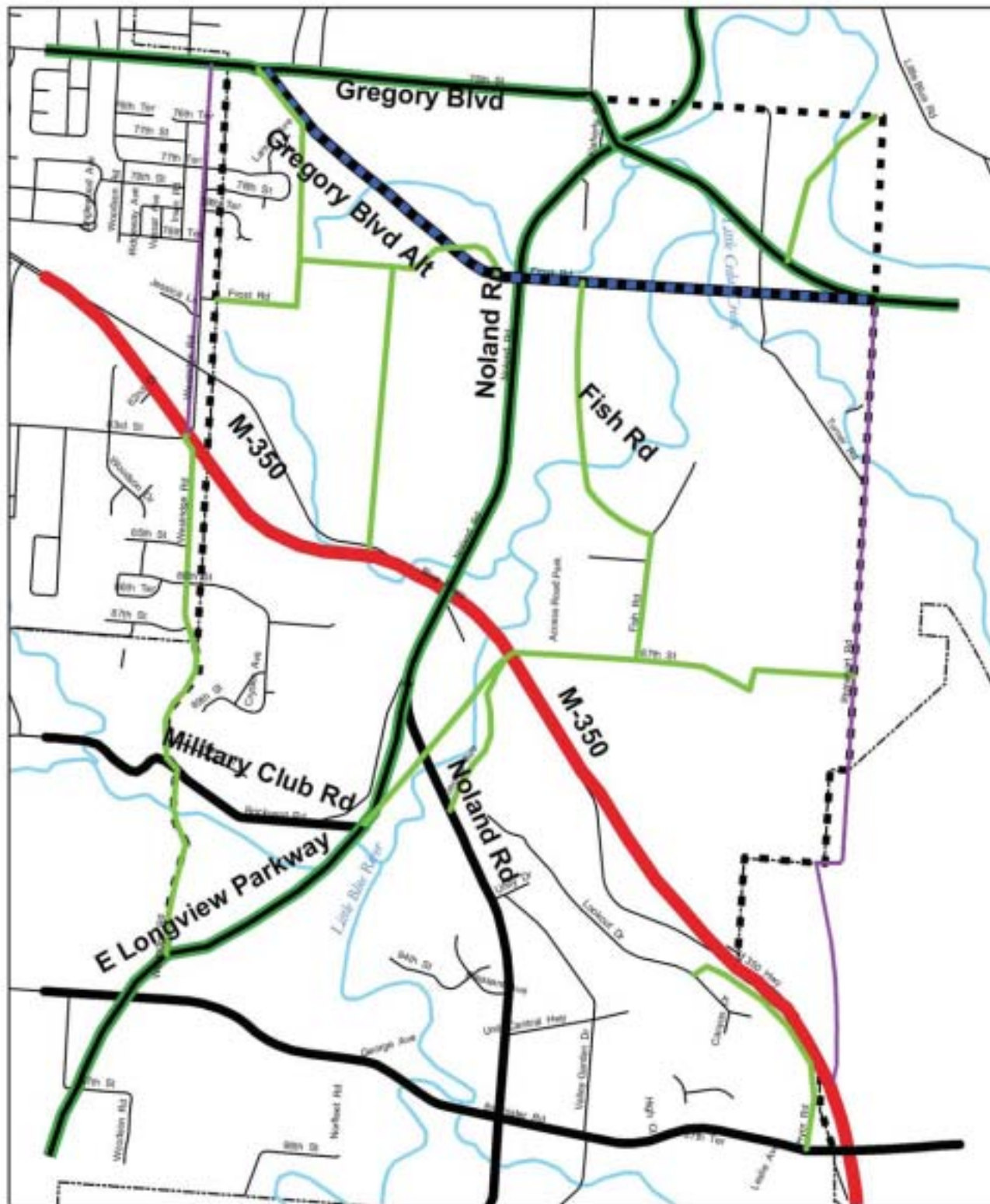


Figure 7: Proposed Street Network

- Freeway/Interstate
- Parkway/Boulevard
- Primary Arterials
- Secondary Arterial
- Other Knottown Area Roads

- Alternate Alignment for Gregory Blvd
- Proposed Collector
- Knottown Study Area Boundary

NOTE: Capacity analysis is required to determine if collectors should be upgraded to arterial streets.



TRANSPORTATION GUIDING PRINCIPLES

- Plan for and build an “economical” road system to meet existing and future needs.
- New roads and roadway improvements should be strategically targeted to areas that are ripe for development.
- Roads will be brought up to City Standards
- Promote a “balanced” transportation system that considers the needs of vehicles, transit, pedestrians, and bicycles.
- Plan for connections to a regional trail system (Little Blue Trace and Katy Trail) in Knottown and provide connections as development occurs.

ROADWAY PRIORITIES AND RECOMMENDATIONS

PRIORITY AREAS

- Primary (1) – Existing developed areas within a development priority zone.
- Secondary (2) – Developing or redeveloping areas contiguous to existing development or on the fringe of the development priority zone.
- Low (3) - Greenfield areas generally not within a development priority zone; transportation improvements are considered long term projects.

RECOMMENDATIONS

- Develop improvements to 350 Highway including the following:
 - » Widen 350 Highway at the existing railroad overpass west of Noland Road and improve the intersection of Noland Road and 350 Highway to include additional turn and/or through lanes and improved signalization.
 - » Coordinate with the City of Raytown for improvements to the intersection of Westridge Road, 83rd Street with 350 Highway. Potential improvements may include relocation of Westridge Road at East 83rd Street Drive or consolidation of 350 Highway east and west-bound lanes away from the intersection.
 - » Realign 350 Highway between Westridge Road and Noland Road as recommended in the 350 Highway Corridor Plan. Consolidate both east and west bound lanes on 350 Highway to the south side of the existing highway. Former westbound lanes to become a collector street. (Long Term)
 - » Realign 350 Highway east of Noland Road as recommended in the 350 Highway Corridor Plan. Consolidate both east and west bound lanes on 350 Highway to the north side of the existing highway. Former eastbound lanes to become a collector street. (Long Term)
 - » Implement curb and storm sewer improvements on the western segment (between Raytown City Limits and Noland Road).

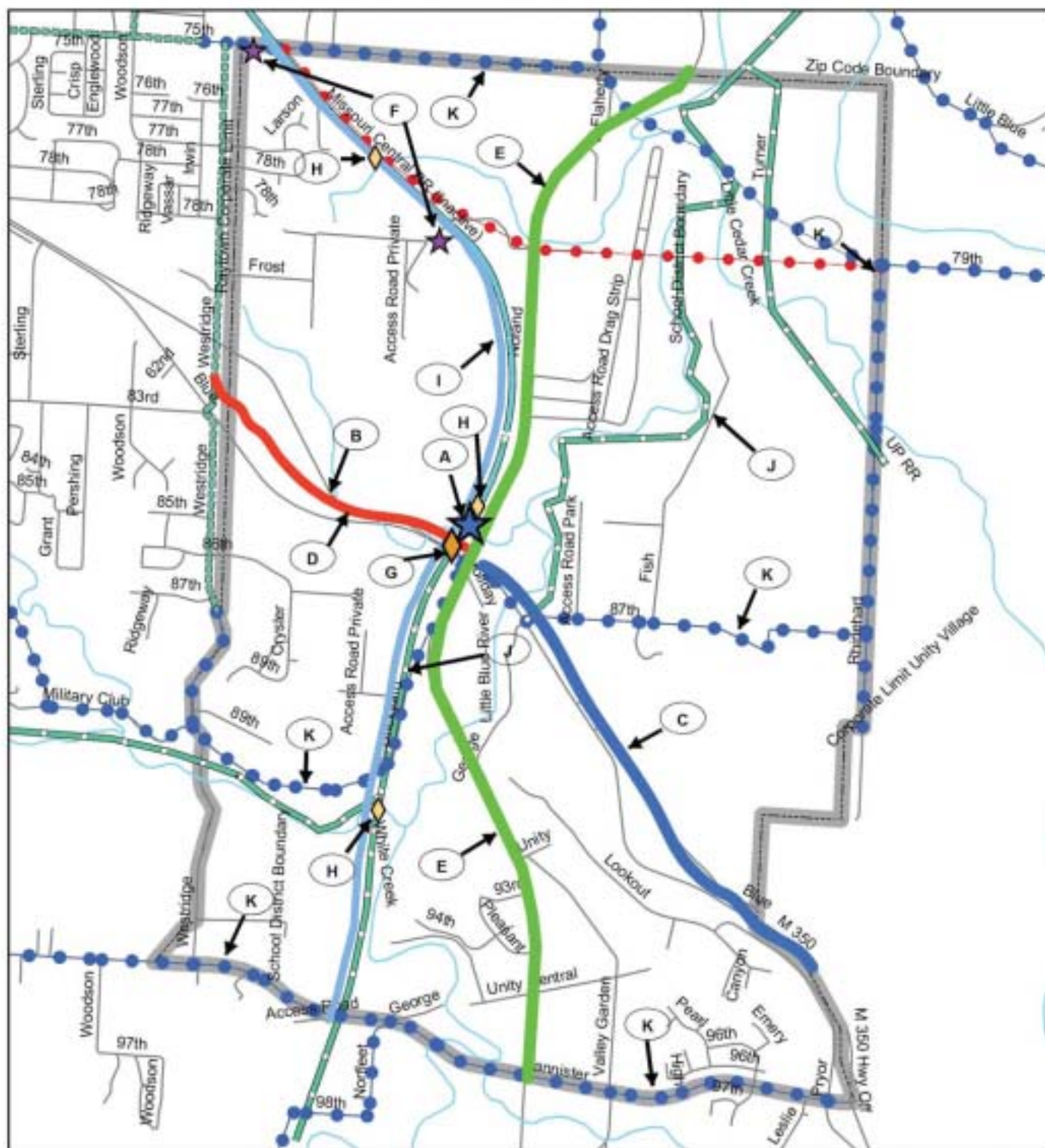


Figure 8: Possible Transportation Projects
(priority)

- A - Widen M-350 at Noland Rd Overpass/ Intersection Improvements (1)
- B - Realign M-350 between Westridge and Noland Rd (3)
- C - Realign M-350 east of Noland Rd (3)
- D - Curb and Storm Sewer M-350 between Westridge and Noland Rd (1)
- E - Expand Noland Rd to 4 lanes (3)

- F - Removal of all three Railroad Bridges (2)
- G - Pedestrian Bridge over M-350 near Noland Rd (1)
- H - Trails - three pedestrian bridges over streams (1)
- I - Crushed Rock Trail on old Railroad Bed (1)
- J - Metro Green Crushed Rock Trail (2)
- K - Establishing On Street Trails along various roads (2)

- Improve Noland Road to a four lane cross-section (more lanes dependent upon more detailed study).
- Carefully manage access to all proposed arterial streets and freeways.
- Develop a local road system as land redevelops, to include:
 - » An additional connection between 87th Street and Noland Road, connecting to Fish Road.
 - » To the southwest of 350 Highway, aligning George Road to Brickyard at Noland Road would provide a good east-west connector in the southern portion of the study area.
- Conduct a detailed alignment study of future Gregory Boulevard/75th Street including estimated costs and recommended phasing. Consider initial improvements to the 75th Street portion adjacent to the development priority zone.
- Conduct a detailed alignment study of future East Longview Parkway including estimated costs and recommended phasing.
- Perform capacity analysis based on the future development depicted on the Land Use Plan to determine the total number of lanes that will be required on major streets. The capacity study should also determine if proposed collector streets should be designated as arterials. (See Figure 7)
- Explore the removal and/or widening of the existing railroad bridges over 75th Street, Frost Road and 350 Highway. The future utilization of this corridor as a trail should be considered along with any plans to remove the railroad bridges to improve vehicular circulation.
- Implement a system of highly connected collector streets within new developments to be constructed as development occurs. A connected collector system will help alleviate potential capacity issues on arterial streets.



Railroad bridges should be widened or removed to improve vehicular circulation and safety.

TRANSIT RECOMMENDATIONS

- Work with the Kansas City Area Transit Authority (KCATA) to establish additional bus stops for existing routes in the area such as at Noland Road and 350 Highway for the 152x Route.
- The City will work with KCATA to determine if additional transit service along Noland Road, north of 350 Highway, should also be considered as development occurs.
- Work with the Mid America Regional Council (MARC) and KCATA as the Smart Moves Regional Transit Plan is implemented to identify park and

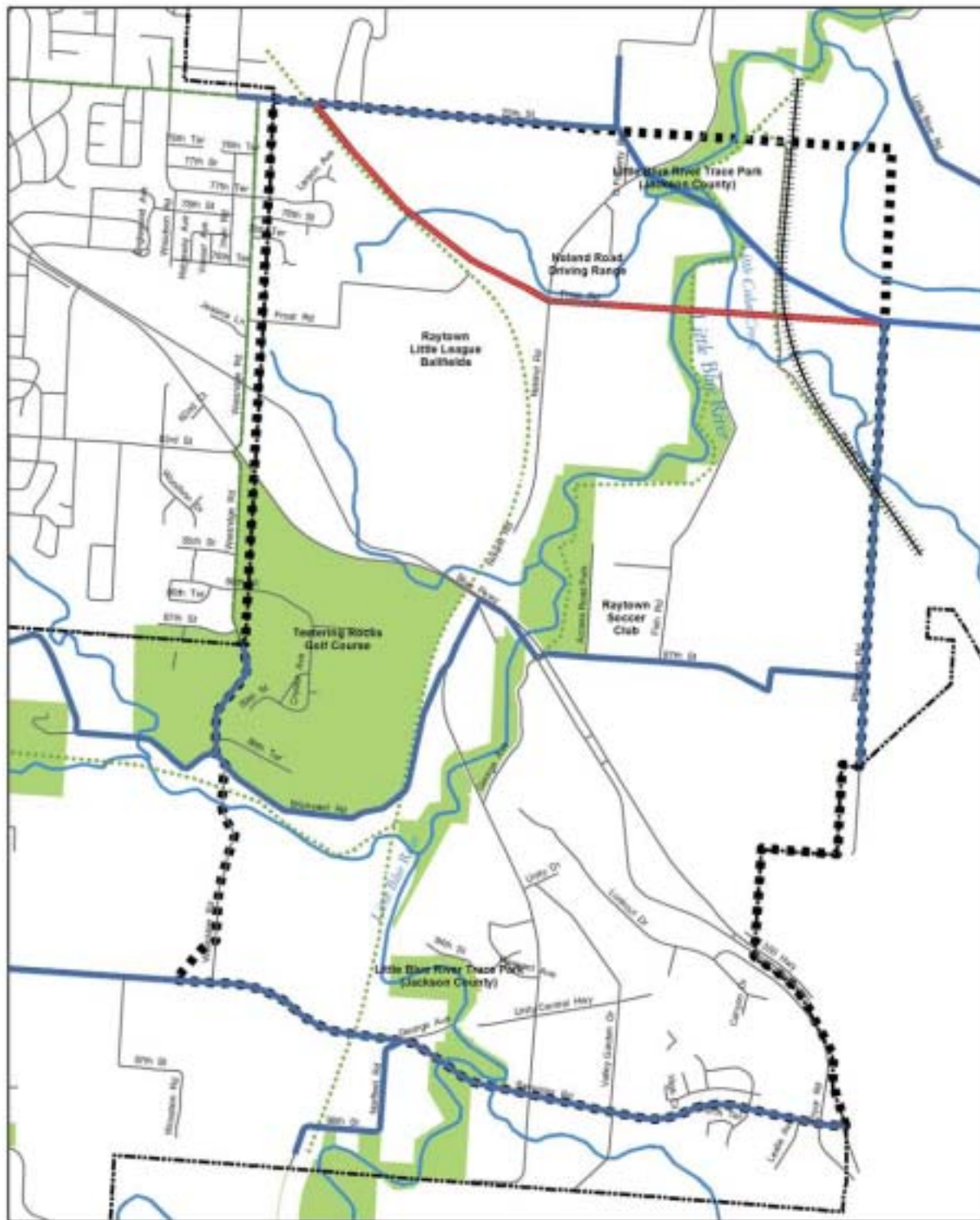


Figure 9: Proposed Trails and Bike Routes



- Bicycle Routes
- Alternate Bicycle Routes
- - - Greenway Routes
- Routes Outside City of KCMO limits
- Knobtown Study Area Boundary
- Streams
- Parks

ride locations within Knobtown to support the proposed Freeway Flyer route along 350 Highway.

- Major Transit Centers are identified to support future Transit Oriented Development (TOD). TODs encourage a mix of uses and design that compliment and encourage public transportation. Each of the mixed-use nodes identified on the Land Use Plan are recommended to be developed in this manner:
 - » Westridge Road and 350 Highway
 - » Knobtown node at Noland Road and 350 Highway
 - » Bannister Road and 350 Highway

TRAIL AND BICYCLE PRIORITIES AND RECOMMENDATIONS

The map shown on the facing page identifies the ultimate trail and bicycle network for the Plan Area. This map is based on the approved bike route network, the Metro Green Regional Trail Plan and through an analysis of existing conditions. A description of these recommendations follows.

TRAIL AND BICYCLE PRIORITIES

- Primary - Construction of regional trail segments such as the Little Blue Trace and Katy Trail Connector.
- Secondary – Trail connections from new development projects to regional trail corridors.
- Low – Implementation of an on-street bike route system as streets are improved and development occurs

TRAIL AND BICYCLE RECOMMENDATIONS

- A system of on-street and off-street bicycle and multi-use trails should be provided to balance transportation and recreation needs (see Figure 9: Proposed Trails and Bike Routes Map).
- The Plan Area will be interconnected through a series of local trails which will connect neighborhoods to parks, schools, churches, retail areas, and regional trails.
- All trails within the Plan Area will include enhanced pedestrian amenities including way finding signage, lighting, benches, trash receptacles, and generous landscaping.
- Explore a connection to the Katy Trail along former Union Pacific Railroad corridor. Explore off-street trails along White Oak Creek and Little Cedar Creek.



Trail examples:



PUBLIC spaces

Introduction

The physical appearance of streets, sidewalks and public spaces help shape the public's perception of an area. This chapter provides a framework for prioritizing and targeting improvements within the public spaces and rights-of-way and provides general design criteria to preserve the distinctive rural atmosphere of Knobtown.

Key Issues

- Poor maintenance within right-of-way of existing roads and in existing public spaces.
- Negative images created by deterioration of abandoned structures and vacant properties.
- Lack of a pedestrian friendly environment due to high traffic capacity and speeds.
- Lack of any type of identity for the Knobtown Area.
- Lack of landscaping and coordinated or unifying elements on streets.

Public Spaces Guiding Principles

IMPROVE THE APPEARANCE OF KNOBTOWN'S STREETS, BRIDGES, AND PUBLIC SPACES

- Focus on improving appearance of the Knobtown Mixed-Use node and key "Image" streets. Design enhancements will recognize and balance the needs of pedestrians, bicyclists, motorists, and transit.
- Create a visually contiguous roadway system through design that respects surrounding land uses.
- Remove and/or reduce visual clutter such as billboards and utility poles.
- In new developments bring streets up to City standards including sidewalks and curbs/gutters where appropriate.

REINFORCE AND ENHANCE KNOBTOWN'S RURAL CHARACTER

- Promote Knobtown's rural identity by reflecting its history, landmarks, and environmental values through the incorporation and preservation of open spaces and natural landscaping in public spaces.
- Maintain natural uniqueness through the addition of street trees, median plantings, street lighting, reasonable signage control, burial of overhead utility lines, and promote the protection of public viewsheds.
- Integrate unifying design elements to establish the area's identity.

The Urban Design Framework Map, shown on the facing page, represents areas where aesthetic improvements should be focused.

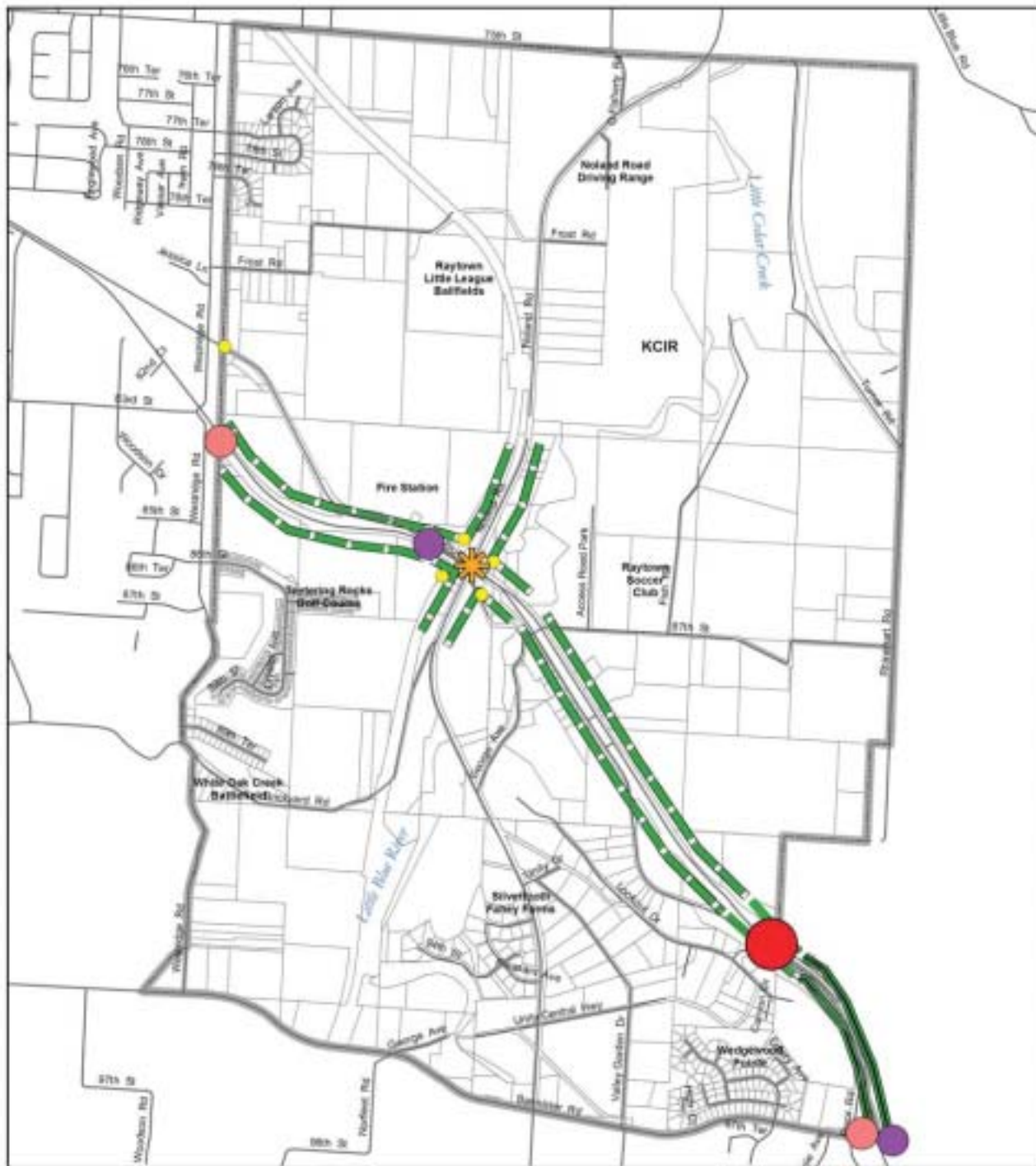


Figure 10: Urban Design Framework



- | | |
|--------------------------|----------------------------------|
| Intersection Enhancement | Bridges |
| Minor Gateway | Light Enhancements |
| Major Gateway | Roads |
| | Rural/Suburban Expressway Street |
| | Transition Streetscape |
| | Urban Expressway Landscaping |

Streetscape and Landscape Recommendations

- Preserve existing stone outcroppings and topography throughout the entire area when establishing other aesthetic improvements, such as street trees, landscaping, and lighting.
- Focus streetscape improvements and enhancements to Primary Image Streets (Noland Road and 350 Highway). Primary Image Streets help set the “tone” of the area by establishing visual and aesthetic standards. Aesthetic enhancements should be a priority on these corridors.
- Streetscape Recommendations by Area (See Figure 10: Urban Design Framework for locations):

MIXED-USE NODES

- » Street trees should be spaced approximately 30-ft apart.
- » Ornamental trees are clustered at major intersections such as Noland Road and 350 Highway.
- » Tree species in this area should be columnar with an open canopy and be limbed up to a reasonable height to ensure appropriate visibility to businesses and related signage.
- » Include gateways at intersections as recommended below. Include secondary gateways or enhancements at entry points into each district.
- » Provide a landscape buffer between the street and sidewalk for street trees and street and pedestrian lights.
- » A variable width, landscaped median at major intersections (where feasible).
- » Light fixtures should be spaced to ensure they meet or exceed the City’s lighting standard.
- » Banners on the street lights will highlight the corridor-wide identity; banners on the pedestrian lights will highlight the mixed use district.
- » Decorative stamped paving or a change in paving material and/or paving color will demarcate crosswalks.



This rendering depicts possible streetscape improvements to the intersection of 350 Highway and Noland Road. The current conditions are pictured at left.



This rendering is conceptual and is meant to show general roadway character and appearance. This illustration is not meant to show exact building design or site layout.



RURAL/SUBURBAN EXPRESSWAY

- » Regular street tree spacing on curb side and combination of shrubs and trees planted in median area.
- » Special pavers at major intersections and pedestrian crossings.
- » Provide enhanced street lighting.
- » Unifying elements, such as banners.
- » Establish street trees and aesthetic street lighting within the median extending from the Little Blue River to the east highway split.
- » Utilize natural plantings; informal plantings enhance the general driving experience and frame views along the rural segments of 350 Highway.
- » Preserve rural look with open ditches, shoulders and no curbs.
- » Utilize wide medians for plantings.

TRANSITION SEGMENT

- » Design a landscaping concept that provides a seamless transition between the Rural Expressway segment and the Urban Expressway segment.
- » Establish a gateway with monumentation, a fountain, and landscaping on 350 Highway facing eastbound lanes just to the west of Unity Village near the city limit sign.

URBAN EXPRESSWAY

- » Use MODOT enhanced expressway landscape standards.
- » Emphasize low maintenance.
- » Additional landscaping enhancements at interchanges.
- » Use simple, large scale plantings and massing.
- » Utilize and preserve existing high quality natural trees and plants.





- Street Lighting Guidelines:
 - » Focus enhanced, aesthetic street lights with banners at the Westridge Road/350 Highway and the Noland Road/350 Highway mixed-use nodes. Utilize low maintenance, off the shelf lighting. Additional enhanced street lighting (spacing will be determined by lighting level requirements) should be provided along 350 Highway in all remaining segments.
 - » All light fixtures should be designed to shine down onto the street and should utilize reflectors to shield excessive glare and prevent light from spilling into adjacent developments and neighborhoods.
- Pedestrian Lighting Guidelines:
 - » Focus pedestrian lighting to mixed-use nodes along Primary Image Streets.
 - » Pedestrian lighting will be provided to illuminate nodes of activity along Primary Image Streets, trails, park frontages, public plazas, courtyards and activity areas.
 - » Pedestrian lighting fixture styles should complement the rural character of the area.
 - » Lighting fixtures should be made of durable materials and designed for low maintenance requirements.
- Street Tree Guidelines:
 - » Selection of tree species including ornamental trees are to be approved by the City Arborist.

Gateway and Intersection Enhancements

Establish a hierarchy of gateways in the Knobtown area (see Urban Design Framework Map) along Primary Image Streets, important intersections, corners, thresholds and municipal boundaries:

- Major Gateways - large scale focal points generally located at the edge of the Plan Area or at the intersections of primary image streets.
- Minor Gateways - smaller than Major Gateways and generally highlight particular districts, corridors of distinction and neighborhoods. These gateways should reinforce the individual district or neighborhood's unique character through imitation of the size, scale, materials and overall character of each area.
 - » Each Mixed-Use District should have a Minor Gateway at each entry point.
 - » New residential developments should have a neighborhood gateway at major entry points.
- Gateways and Intersection Enhancements should include vertical architectural features or focal points constructed of high-quality

materials such as stone, cast stone, tile, metal, or masonry and a combination of the following elements:

- » Landscaping
 - » Plazas with pedestrian amenities such as seating, shade, and triangulation elements
 - » Water features
 - » Decorative lighting, walls or fencing
 - » Monument-style signs, if appropriate, with landscaping to announce district or municipality
- Incorporate gateway design with overall streetscape design where appropriate.
 - Develop high impact intersection enhancements at nodes and intersections along primary image streets. Reserve areas at major intersections for enhancements such as gateways and landscaped focal points (see Development Guidelines).
 - Incorporate pedestrian crossings into gateway design.
 - Secondary gateways and intersection enhancements on private property may be different but should complement the design of major gateways and streetscape improvements.

Bridge Enhancements

- Utilize existing bridges on Primary Image Streets for applied enhancements.
- Each bridge should employ a unique design.
- Remove the railroad bridge across 350 Highway to satisfy stormwater/traffic needs. Construct a new bridge with a focus on the abutments as a “gateway.”
- Bridge enhancements should be integrated into the overall streetscape design.



Major Gateway Enhancement
Source: 350 Blue Parkway Study



TRIANGULATION ELEMENTS
draw people into public spaces and plazas. These elements can include water features, vendors, performers, or atmosphere.



Source: 350 Blue Parkway Study

development GUIDELINES

Introduction

The physical appearance of development is a major factor in determining public perceptions of an area. Throughout the planning process, area stakeholders expressed a desire for new development to create a sense of place and enhance the area's existing small town/rural character. The following Design Guidelines provide a framework for quality development consistent with the Plan Vision.

FOCUS Kansas City Plan

Quality Places Guidelines

These 'Quality Places Guidelines' will act as the basic framework in the creation of high quality places to live and work. These characteristics will be achieved by implementing the specific guidelines in this chapter.

- Walking is feasible, safe, and inviting.
- Streets form a continuous network, maintain a minimum width, are well designed, and allow for adequate public safety and traffic volume.
- Transportation alternatives are convenient and easy to use.
- Existing valuable and historic buildings are refurbished and reused.
- Buildings are designed to create or contribute to a sense of community.
- Residential land use is an essential component of mixed-use development.
- Neighborhoods and commercial districts have identifiable centers that create places for residents or employees to gather, interact, and communicate, and help create an identity for the area.
- New development occurs at infill sites or contiguous to existing development and uses existing infrastructure effectively.
- Neighborhoods and districts have distinct and identifiable characters.
- Development preserves or creates open space, respects existing topography, and minimizes the impact of development on the natural environment.
- New development and infrastructure are built to be useful for 100 years or more.

Landscape, Screening, and Transitions

INTENT:

- To provide a balance between buildings, impervious surfaces, and landscaped areas through enhancement of streets, parking lots, plazas, open space, buildings, gateways, and other structures.
- To provide screening/buffering between structures, parking lots, pedestrian paths and between developments of varying intensity and scale.
- To provide a physical and visual separation between higher and lower intensity uses by softening and mitigating the impacts of large buildings and paved areas.

LANDSCAPE AND SCREENING GUIDELINES:

All development and redevelopment proposals should provide a Landscape Plan prepared by a registered landscape architect, which incorporates the following guidelines.

- Screen all trash dumpsters, service areas and loading areas with a combination of landscaping, decorative walls/fences and berms.
- Landscape the interior and perimeter of all parking lots. Screen surface parking lots adjacent to streets with a combination of landscaping, decorative walls/fences and berms.
- Building foundations shall be screened with ornamental plant material, such as trees, flowering shrubs and perennials, and ground covers.
- Guidelines for walls and fences:
 - » Fences along streets should be no more than 4 feet tall.
 - » Walls and fences shall be constructed of high quality materials, such as decorative blocks, brick, stone, naturally resistant wood, and wrought iron.
 - » Walls and fences should be complemented with landscaping.
 - » Where chain link or security fencing are required, vegetation should be used to screen such fencing from view from adjoining streets and development. Plastic slats shall not be used as an alternative.
- Use landscaping to define and enhance the sense of arrival at appropriate site entries, and to visually frame buildings.
- Plant materials shall be suited to a suburban environment and local climate. Native plant materials are encouraged. A mix of evergreen and deciduous plant material should be used.
- Raised planters and/or planting beds should be used adjacent to roadways where landscape is vulnerable to water splash from passing vehicles.



Landscaping in public spaces can greatly contribute to the character and identity of an area.

TRANSITION GUIDELINES:

- Transitions should be provided between changes in use or intensity of use.
- Architectural Transitions and Green/Open Space Transitions should be the primary transition technique, supplemented with landscaping and screening transitions.

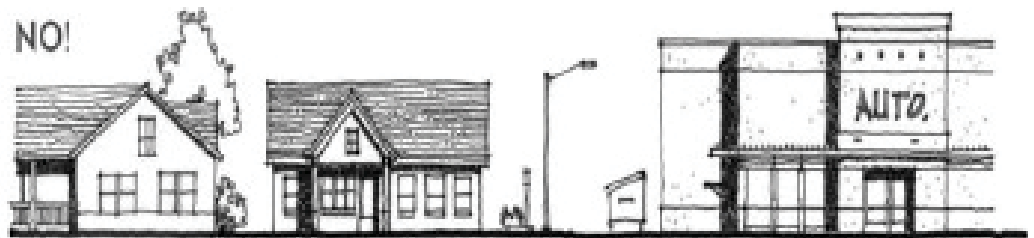
Architectural Transitions include:

- » Using similar building setbacks, height, and roof forms.
- » Mitigating larger building mass with façade articulation
- » Reducing building heights, intensity of use and densities as development moves closer to low intensity areas

Green/Open Space Transitions include:

- » Small green spaces, courts, squares, parks, or plazas
- » Existing natural features, including changes in topography (not retaining walls), streams, existing stands of trees, etc.
- A combination of landscaping, walls, fences and/or berms should be used where other transition tools are not possible, or where the City determines other transition tools by themselves are not adequate.
- Connections between developments should be incorporated into transition design.
- Provide landscape transitions between developed and natural areas.

Mixed land uses without similar building materials and no landscape transition.



Mixed land uses with landscape buffer and similar construction styles.



Open Space

INTENT:

- To provide well defined natural and developed open spaces as amenities that serve as the focus of block, lot, and circulation patterns.
- To supplement public open space with privately developed open space that completes linkages by integrating public gathering spaces and open spaces within new development.

OPEN SPACE GUIDELINES:

- All areas not occupied by buildings, structures, streets, driveways and parking lots should be identified as “public spaces.”
- Open spaces must be integrated into the development design, and not placed on undevelopable remnant or unusable perimeter buffers.
- Provide small developed open spaces embedded in new development that serve as an amenity and create value (a neighborhood green (park) in residential areas and public plazas or landscaped gathering spaces in commercial areas).
- All open space areas must be accessible from a public street, sidewalk or trail.
- Design stormwater management areas as attractive water feature amenities or focal points.
- Set back buildings, parking, and grading from significant natural features to ensure their continued quality and natural functions.
- Open space areas should be visible, safe, attractive and inviting by incorporating pedestrian lighting, public art, landscaping, benches, and other amenities.

Environmental and Stormwater Management

INTENT:

- To protect the existing environmental and cultural assets of the area through increased stormwater infiltration, reducing flooding and improving water quality.

ENVIRONMENTAL AND STORMWATER MANAGEMENT GUIDELINES:

- Retain the natural and visual character derived from topography, woodlands, streams, and riparian corridors. Hills and natural slopes should be preserved and excessive cuts and fills should be avoided.
- Provide greenway corridors to preserve natural drainage areas, floodplains, slopes over 15 percent and wooded areas.
- Provide a tree survey and Preservation Plan with the development application. Replace trees over 10 inches in caliper to be removed by construction.



Whether urban or rural in character, open spaces should be easily accessible to promote interaction between a variety of people and land uses.



Measures should be taken to manage and improve stormwater in the area. These include bio-swales, detention, and filtering areas.



Sidewalks, trails, and other pedestrian and bicycle circulation areas should provide amenities that provide convenience for both walkers and riders. These amenities include shade, benches, trash receptacles, and bike racks.

- Views of rivers and natural features should be preserved and integrated into developments (see Figure 10: Urban Design Framework). Complete removal of trees to create views is discouraged.
- Provide detention and use non-structural stormwater BMPs to preserve open space within and between developments, and provide storm water treatment.
- Design detention areas as amenity features. Use graduated spillways on detention facilities.
- Limit stormwater runoff from new developments to predevelopment levels.
- Green roofs and pervious pavers are encouraged to mitigate stormwater runoff.

Pedestrian and Bicycle Circulation

INTENT:

- To provide a safe, direct, continuous, convenient, inter-connected, and visually pleasing system of pedestrian walks, bikeways, and bike routes.
- To provide a pedestrian/bicycle network with the same or higher priority as vehicular traffic.

PEDESTRIAN AND BICYCLE GUIDELINES:

- Projects abutting the Little Blue River or a regional trail corridor should incorporate pedestrian and bike connections to these amenities.
- Provide pedestrian walkways which connect the primary building entry to the street sidewalk by the most direct route.
- Walkways should link all buildings, open space and parking areas within a development and link to adjacent streets, development and open space systems.
- Provide pedestrian walkways and sidewalks along all internal streets/drives which extend to sidewalks along perimeter streets.
- Walkways should extend through all parking areas within linear landscape strips to define pedestrian paths.
- Where a walkway crosses a parking lot, street or driveway, it should be clearly delineated by a change in paving materials, color, texture, or height.
- Provide pedestrian and bicycle connections where automobile connections are not feasible.
- Provide on-site bicycle parking areas in visible, active, well lit areas near building entries.
- Minimize street crossing distances and meet minimum levels of service as recommended in the Kansas City Walkability Plan.

- Set back sidewalks from the street and include a lawn for trees between the curb and the sidewalk. Allow sidewalks closer to the street curb in commercial or mixed-use areas and incorporate tree planters and landscape when sidewalks are adjacent to the curb.

Site Planning

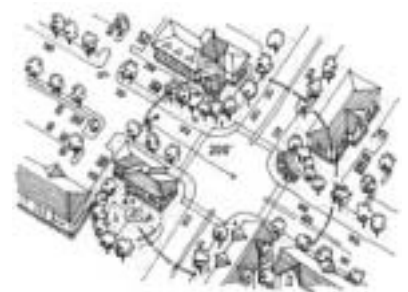
INTENT:

- To utilize building placement and open space to establish uninterrupted views, ensure streets are treated as development frontage, and provide for compatibility of use, access, and circulation between adjoining properties.
- To provide special definition of streets at key locations such as image street and arterial street intersections or area gateways and maximize the positive character of streets and buildings through continuity of architecture and landscape frontage.

SITE PLANNING GUIDELINES:

(ALL RESIDENTIAL, COMMERCIAL, AND MIXED-USE DISTRICTS)

- The rural character of the area should be maintained by preserving existing wooded areas, using extensive landscaping and minimizing curb cuts.
- Streets should form a network of regular intersections and neighborhood connections. Continue streets through to as many neighborhoods as possible and allow for future connections where topography permits.
- Streets should follow natural contours to minimize the impact on the natural terrain.
- Streets should be the minimum width practicable and should accommodate pedestrians, bicyclists, and automobiles.
- Avoid cul-de-sacs when through street connections are not desirable. Provide streets parallel to open space or looped streets with neighborhood green space.
- Use streets to define the boundaries of natural or developed open space.
- Locate parking, service areas, and vehicular circulation behind or to the side of buildings and not along primary street frontage(s).
- Front buildings onto a street or major access drive to create a clear street edge and to provide physical definition of roadways.
- Corners of major intersections should provide a focal point within a 200-foot radius of the center of the intersection and around gateway areas, such as a vertical architectural feature, public art, and/or public plaza.
- Parking areas should not be located within a 200-foot radius of the center point of a major street intersection or gateway, unless located behind a building.



Intersection Focal Point

RESIDENTIAL AND MIXED-USE SITE PLANNING GUIDELINES:

- Provide rear alley/rear garage access for attached residential structures in medium density residential areas and mixed use areas.
- Avoid direct driveway access on major streets for residential properties, except for residential estate lots.
- Utilize T-intersections in locations where it is desirable to highlight important public spaces, open space areas or views.
- Private drives should be designed similar to public streets with sidewalks, planting strips, street trees, and lighting.
- In residential areas, allow alternative local and collector street designs with vegetated swales in lieu of enclosed storm water systems.

COMMERCIAL AND MIXED-USE SITE PLANNING GUIDELINES:

- Mixed-Use Districts will have a unique character or sense of place with an identifiable center that includes defined “people places” for residents, shoppers, workers, and visitors to gather, interact, and recreate and provide opportunities for housing choice and variety.
- Mixed-use Districts will develop as multi-modal hubs. Development densities for commercial, office and residential uses will be greater within these areas to encourage transit use.
- Mixed-use Districts should be considered for a “Pedestrian Street” zoning designation.
- Parking requirements should be decreased within these areas to encourage transit ridership.
- Auto-oriented uses such as drive-through restaurants are discouraged in mixed-use areas.
- Provide a tight network of bicycle and pedestrian friendly streets, wide sidewalks, benches, street trees and landscaping, buildings oriented toward the street, and on-street parking.
- Locate buildings in mixed-use areas along a build-to-line with parking located predominately behind buildings. No parking or vehicular circulation should be located between the building and street. A minimum of 75% of buildings should have a zero foot setback.
- In commercial (general office and retail) areas, limit the amount of parking and vehicular circulation located between the building and the street.
- Nonresidential freestanding buildings should be clustered to define the street edge and create plazas or public gathering spaces between buildings.
- Frame and enclose parking areas with buildings on at least three sides. A majority of the frontage along adjacent streets should be occupied by buildings, decorative architectural walls or landscaping.



Commercial and Mixed-Use areas should have a distinct character and should provide for pedestrians, bicyclists, and vehicles.

- Primary sidewalks in mixed use community district areas should be as wide as practicable but no less than eight feet wide. Secondary sidewalks should be a minimum five feet wide.
- Increase sidewalk width when adjacent to on-street parking and include a “transition zone” of pedestrian amenities along the street including street trees, landscape planters, pedestrian lighting, and other streetscape amenities.

Architectural Character

INTENT:

- To create a built environment that is in scale and character with pedestrian activities to ensure high quality appearance, form, and scale of buildings to enhance the character of the area and provide long term value.
- To minimize the dominance of garages on residential streets.

ARCHITECTURAL CHARACTER GUIDELINES (ALL DISTRICTS):

- The rural character of the area should be maintained by using rural materials in construction, such as native stone.
- Architectural materials should compliment the character of the existing built environment through use of high quality, durable materials. Suggested materials include: wood, masonry, concrete, stone, cast stone, and tile.
- Use of stucco board, vinyl and metal siding is discouraged:
- Buildings facing Primary Image Streets should integrate architectural details to enhance the street edge and promote human scale and interest. Suggested materials and details include but are not limited to corner elements, awnings, window inlets, planted window boxes and articulated entries.
- Use building placement and design to define roadways as civic spaces.
- Design buildings to relate directly to the street using the following techniques:
 - » All building frontages visible from a street or a residential area shall have the equivalent treatment of the primary building facade.
 - » Provide a primary entry for building facades facing arterial streets, or a facade treatment of comparable architectural, material, and detailing quality.
 - » Minimize long expanses of wall at a single height or in a single plane.
 - » Vary floor heights to follow natural grade contours if significant variation is present.



New developments should have a character that is compatible with that of the existing area.

- » Use the highest architectural detailing when located near a major intersection or primary image street.
- Design buildings to provide human scale, interest, and variety using the following techniques:
 - » Use the highest level of architectural detail near streets and entries, and around the ground floor.
 - » Vary building mass, height, profile, and roof form that provide human scale while maintaining a consistent overall building form to the street edge.
 - » Vary building form with recessed or projecting bays and changes in materials, details, surface relief, color, and texture.
 - » Expression of architectural or structural modules and detail.
 - » Diversity of window size, shape, or patterns that relate to interior functions.
- Provide complementary variations in building form, unless the area is designed in a manner that relies on uniformity to establish an architecturally pleasing pattern.
- Provide windows, doors, plazas, and other features on building facades adjacent to open space to encourage pedestrian activity and provide visual oversight.

RESIDENTIAL ARCHITECTURAL CHARACTER GUIDELINES:

- Residential developments shall avoid monotonous rows of garage doors visible from the street.
- Provide residential dwelling designs with alternatives to street oriented garages, such as a mixture of rear and side loaded garages, attached and detached garages, carports, and porte cocheres.

COMMERCIAL AND MIXED-USE ARCHITECTURAL GUIDELINES:

- Primary building facades should be parallel to the sidewalk.
- Where buildings are set back from the sidewalk, such areas should be treated as public spaces such as a plaza or courtyard.
- The front of all mixed-use buildings should include pedestrian-oriented elements such as transparent display windows, outdoor seating for dining areas, public art, and pedestrian amenities such as fountains and benches.
- Locate and design large buildings to minimize windowless walls and service areas visible from public streets.
- Provide ground floor retail with direct pedestrian entries oriented toward public streets, parks, or plazas. Primary entries must be easily accessed and directly visible from a street.



New residential development should consider garage placement on the side or rear of dwellings as an alternative to the typical street oriented design.



- Include a repeating pattern on building facades that includes color change, texture change and material change, with at least one of the elements repeating horizontally.
- Outside sales, storage, or display areas are discouraged. When permitted, such areas shall be screened with landscaping or enclosed with materials integral to the building architecture.
- Provide a clear and consistent street edge with at least 50% of the building's active wall oriented toward the street. An active wall is the side of the building containing the majority of the storefronts, customer entrances, and windows.
- Provide no less than 20% window to solid wall area for portions of a building facade above the ground floor.
- Incorporate transparent glazing at all occupied levels of building facades oriented toward streets and pedestrian areas.
- Provide arcades, display windows, entry areas, awnings, and other features along no less than 60% of the ground floor facing public streets.
- Roof form, material, color, trim, and lighting should be an integral part of the building architecture. Roofs should not serve as attention-getting devices for signage or as an identifiable corporate image.
- Locate drive-through facilities, when permitted, on the side or rear of a building away from a street.

Signs

INTENT:

- To provide a clear, easily understandable, coordinated method of conveying information for businesses and address identification.
- To ensure that signage is unobtrusive and integrated with the building and/or landscape design.

SIGN GUIDELINES:

- Signs should be visible and clearly legible for both the pedestrian and motorist.
- Signs should be highly graphic with a minimum number of words.
- Freestanding pole/pylon signs are not allowed within new developments. Monument signs are encouraged.
- Signs must not obscure important architectural features.
- Moving, revolving, and flashing signs are discouraged.
- Do not approve new off-site advertising signs, and remove existing non-conforming signs whenever legally possible. Remove existing billboards if public funds are used for a project. The practice of substitution and/

Alternatives to the popular pole/pylon signs:



Wayfinding signs assist pedestrians in commercial and mixed-used areas, and should also comply with signage guidelines.



or relocation of billboard signs is discouraged.

- Monument signs should be landscaped to complement the existing or proposed landscaping of the project.
- Non-standard sign forms are encouraged.
- Signs should be consistent with the design, materials and colors of the overall development.
- Signs should be made of high quality and durable materials such as brick, stone, metal or heavy-weight plastic.
- When floodlights are used to illuminate signs, they should be designed to eliminate glare, and spill over onto adjacent properties
- Low-scale signs such as wall signs, awning signs and building signs should be used for retail and office uses.

IMPLEMENTATION



Introduction

The intent of the implementation chapter is to establish a strategy to implement the policies of the Plan. This chapter will evaluate potential actions discussed in the Plan, provide cost estimates, and outline the path necessary to fund and complete projects. Portions of the chapter include:

- Project Prioritization
- Implementation Committee/Guidelines
- Decision-Making Criteria
- Implementation Matrix

Potential Phasing/Prioritization

Development priority zones have been established through public input during the planning process. These areas were identified after an evaluation of major constraints and planning issues, such as environmental areas, lack of adequate infrastructure, slope and topography issues, and roadways. These areas serve as target areas for public investments in the Knobtown Area. Any streetscape improvements should have a maintenance fund for amenity upgrades. Below is a summary of projects which have been identified within the priority development areas (See Figure 1).

PRIORITY ONE -- 1-5 years
PRIORITY TWO -- 6-10 years
PRIORITY THREE -- 11+ years

PRIORITY ONE PROJECT LISTING

The priority one area includes areas north and south of 350 Highway east to the Noland Road intersection. The establishment of a Knobtown Node at the 350 Highway/Noland Road intersection was viewed as a priority project. In order to create this node, a number of different projects would need to be completed:

- Initiate a Sewer Plan to examine potential phasing and funding sources of expanding sanitary sewer into the Knobtown Area. Include an examination of the feasibility of re-using the sewer main that generally follows Noland Road from Gregory Blvd/75th Street to Brickyard Road.
- Stormwater infrastructure improvements at the 350 Highway/Noland Road intersection.
- Streetscape improvements along 350 Highway from Westridge Road to Rhinehart Road.
- Establishment of a gateway at Westridge Road and 350 Highway.
- Establishment of a gateway on 350 Highway at Unity Village.
- Railroad bridge widening or removal above 350 Highway west of Noland Road.
- Intersection improvements and enhancements at 350 Highway and Noland Road.

A BRANDING STUDY will investigate the aspects that contribute to the overall character of the Knobtown Area.

- Work with the City of Raytown for improvements to the intersection of Westridge Road, 83rd Street with 350 Highway.
- Detailed alignment study of future Gregory Boulevard/75th Street.
- Detailed alignment study of future East Longview Parkway.
- A comprehensive traffic capacity study analysis based on the future development as depicted in the Land Use Plan.
- Exploration of a connection to the Katy Trail along the former railroad corridor.
- Remove the railroad bridge at Frost Road to improve safety and traffic circulation.
- Remove the railroad bridge at 75th Street to improve safety and traffic circulation.
- Capacity and safety improvements to Noland Road.
- Complete a “Branding” study to explore ways to improve the area image.

PRIORITY TWO PROJECT LISTING

The priority two area includes, but is not limited to, 350 Highway and land to the south which has already been mostly developed. The following projects are recommended for this area:

- Further expansion of infrastructure into existing developments. (See Figure 5)
- Expansion of infrastructure into undeveloped areas contiguous to priority one areas.
- Stormwater infrastructure improvements (based on the KCONE Stormwater Management Plan).
- Explore a connection to the Katy Trail along the former Union Pacific Railroad corridor or the Little Blue River.
- Establishment of on-street bicycle routes.

PRIORITY THREE PROJECT LISTING

Priority three areas are not included in the Development Priority Zone, but include projects viewed as low priority during the planning process. Due to high cost and the long-term implementation of these projects, it is recommended to complete individual feasibility studies.

- Realignment of 350 Highway.
- Further expansion of infrastructure into undeveloped areas. (See Figure 5)
- Direct connection between 87th Street and Brickyard Road
- Extension of Fish Road from 87th Street north to Gregory Boulevard/Noland Road

Implementation Committee

The driving force behind the implementation of the recommendations in the Knobtown Land Use and Development Plan will include business owners, residents, property owners, and any other interested citizens. This group will work with the City and other agencies to oversee implementation of projects recommended in the Knobtown Area. It is recommended that a formal Knobtown Development Plan Implementation Committee (Knobtown Committee) be established immediately after the City Council adopts the Plan. The Knobtown Committee will work with relevant entities to ensure strong public participation throughout the implementation process.

Decision-Making Criteria

In order to have a successful Plan, decision making criteria are established based on the vision identified at the beginning of the process. Decision makers use these criteria as a reference when reviewing development applications or discussing potential projects in the Plan Area. These criteria are listed below.

- Does the proposed development exemplify the rural image and appearance of the Knobtown Area through consideration of the following:
 - » Execution of design standards to enhance adjacent public spaces
 - » Address existing 'sensitive' land uses
 - » Enhance the natural beauty and rural character through preservation of environmental features
 - » Protect and enhance historical features such as the Battle of White Oak Creek site and other registered historical buildings and locations
- Does the proposed development or action improve the transportation network and enhance the multi-modal transportation system by providing:
 - » Safe and adequate motor-vehicle circulation through existing and new developments
 - » Improved connectivity through implementation of the Plan roadway recommendations (see page 26)
 - » Opportunities for multi-modal transportation including bicycles, pedestrian, transit, and trail ways
 - » Connections to the regional trail system
- Does the project improve basic infrastructure by:
 - » Upgrading water mains and sanitary sewer service
 - » Constructing or upgrading sidewalks to improve walkability
 - » Improve safety and capacity of existing roads



IMPLEMENTATION SUBCOMMITTEES

Depending on the specific issue being addressed, it is recommended to form subcommittees.

Subcommittees should be based on chapters of this Plan, such as the Infrastructure Subcommittee, Public Spaces Subcommittee, etc. The group should identify the parts of the Plan that form the basis for their projects.

- Will the proposed development or action provide an efficient, predictable, and orderly pattern of development by:
 - » Adhering to the Plan's land use recommendations
 - » Being contiguous to existing developed areas (within a Development Priority Zone)
 - » Providing adequate water, sewer, and roads to accommodate proposed development
 - » Making logical extensions of existing infrastructure or improvements within the priority areas established in this Plan
 - » Allow for efficient and adequate delivery of service (i.e. police, fire, etc.)
- Will the proposed development or action improve the image and appearance of the area through:
 - » Adhering to the development guidelines
 - » Implementation of streetscape and aesthetic recommendations (enhancement of public spaces)
- Will the proposed development or action protect or enhance the areas historic and environmental resources by:
 - » Adhering to the development guidelines
 - » Preserving natural topographical features and environmental features.
 - » Implementing stream buffer recommendations and stormwater best management practices
 - » Preserving or enhancing historic structures and sites
 - » Protecting viewsheds
- Will the proposed development or action maximize the use of public funds in the area by:
 - » Targeting infrastructure improvements to priority areas
 - » Coordinating multiple improvements concurrently to prevent "digging the same hole twice"
 - » Providing "economical streets"
 - » Provide multiple benefits (i.e. water quality, stormwater management, and recreation/transportation)

Implementation Matrix

The intent of each implementation matrix is to identify a time frame, responsible party, and cost estimates for each prioritized project. Time frames include short term (1-5 years), medium term (6-10 years), and long term (over 10 years). Entities responsible for implementation of the listed projects include the City of Kansas City, the Knobtown Committee, private developers, and partnerships, with the following:

Jackson County, City of Raytown, City of Lee's Summit, Unity Village, Missouri Department of Transportation, Mid America Regional Council, Missouri Department of Natural Resources, KCATA, Historic Kansas City Foundation, neighborhood associations, and other special interest groups.



Cost estimates included in the matrix are based upon project concepts and are contingent upon pre-engineering, surveying, and field analysis to be included in a preliminary Design Plan for each project.

ACTION STEPS	TIME FRAME			RESPONSIBILITY				COSTS
	Short Term	Medium Term	Long Term	City	Knobtown Committee	Private Developer	Partnerships	2007 Estimates
Sewer Expansion Study	●			●	●			\$35,000 area or \$12,000/priority area
Stormwater Infrastructure Improvements		●		●		●		Dependent upon Watershed Master Plan and KC-ONE
350 Highway Streetscape Improvements	●			●	●	●	●	See Table on page 58
KC Gateway at Westridge/350 Highway	●			●	●		●	\$40-50,000
KC Gateway at Rhinehart Road/350 Highway	●			●	●		●	\$40-50,000
Railroad bridge replacement at 350 Highway		●		●	●		●	\$1.6 Million
Widen 350 Highway at over-pass west of Noland Road	●			●	●		●	\$2.4 Million (0.2 miles)
Intersection enhancements at 350 Highway and Noland Road	●			●	●			See Table on page 57
Detailed alignment study of future Gregory Boulevard/75th Street a.) Gregory Boulevard to 75th b.) Gregory Boulevard Alternative			●	●				a.) \$15,000 b.) \$30,000
Detailed alignment study of future East Longview Parkway			●	●				\$20,000
Traffic study (capacity, safety, LOS improvements) of entire area		●		●		●	●	\$40,000

**Cost estimates provided by JEO Consulting Group, INC.

ACTION STEPS	TIME FRAME			RESPONSIBILITY				COSTS
Priority One	Short Term	Medium Term	Long Term	City	Knobtown Committee	Private Developer	Partnerships	2007 Estimates
Removal of railroad bridge at Frost Road to improve safety and traffic circulation			●	●			●	\$500,000
Removal of the railroad bridge at 75th street to improve safety and traffic circulation			●	●			●	\$500,000
Complete a "Branding Study" for a new Knobtown Area image	●				●			\$4 - 6,000

**Cost estimates provided by JEO Consulting Group, INC.

ACTION STEPS	TIME FRAME			RESPONSIBILITY				COSTS
Priority Two	Short Term	Medium Term	Long Term	City	Knobtown Committee	Private Developer	Partnerships	2007 Estimates
Further expansions of infrastructures into existing developements		●		●		●	●	Based upon completion of Priority One sewer expansion study
Expansions of infrastructure into undeveloped areas contiguous to Priority One Areas			●	●		●	●	Based upon completion of Priority One sewer expansion study
Stormwater infrastructure improvements		●		●			●	Dependent upon Watershed Master Plan and KC-ONE
Exploration of a connection to the Katy Trail along the former Union Pacific Railroad Corridor		●		●	●		●	6" crushed rock 3.5 miles \$270,000

**Cost estimates provided by JEO Consulting Group, INC.



ACTION STEPS	TIME FRAME			RESPONSIBILITY				COSTS
Priority Three	Short Term	Medium Term	Long Term	City	Knobtown Committee	Private Developer	Partnerships	2007 Estimates
Alignment Study/ cost analysis of the proposed realignment of 350 Highway			●	●				\$25,000
Construction of Noland Road to four lanes (3 miles)			●	●				\$10.6 million
Infrastructure expansion study into undeveloped areas			●	●		●		\$12,000
Detailed alignment study between 87th Street to Noland Road connection to Fish Road			●	●				\$15,000

**Cost estimates provided by JEO Consulting Group, INC.

Intersection Enhancements

350 HIGHWAY AND NOLAND ROAD

- Concept is based on 5 feet sidewalks with a 5 feet turf/ greenscape buffer.
- Cost estimates do not include irrigation costs.
- Construction cost estimates include contingencies, design and construction engineering, administrative and land acquisition costs.
- All other costs are for the unit price of materials.

ITEM/DESCRIPTION	QTY	UNIT	UNIT PRICE	TOTAL
Benches	8	EA	\$1,200.00	\$9,600.00
Waste Receptacles	8	EA	\$800.00	\$6,400.00
Light Poles	16	EA	\$3500.00	\$56,000.00
Banners	16	EA	\$300.00	\$4,800.00
Ornamental Trees	16	EA	\$300.00	\$4,800.00
Perennials	240	EA	\$25.00	\$6,000.00
Turf	3600	SF	\$0.13	\$468.00
Stamped Paving	4800	SF	\$14.00	\$67,200.00
Build Curb/Widen Road	400	LF	\$75.00	\$30,000.00
Storm Sewer Pipe	400	LF	\$95.00	\$38,000.00
Inlets	400	LF	\$20.00	\$8,000.00
Concrete Sidewalk	800	LF	\$25.00	\$20,000.00
Electrical Conduit	800	LF	\$5.50	\$4,400.00
Construction Costs	-	-	-	\$218,000.00
TOTAL				\$565,268.00

**Cost estimates provided by JEO Consulting Group, INC.

Streetscape Improvements

350 HIGHWAY



ITEM/DESCRIPTION	QTY	UNIT	UNIT PRICE	TOTAL
Trees	495	EA	\$525.00	\$259,875.00
Light Poles	160	EA	\$3500.00	\$560,000.00
Shrubs	2475	EA	\$40.00	\$99,000.00
Banners	160	EA	\$350.00	\$56,000.00
TOTAL				\$974,875.00

**Cost estimates provided by JEO Consulting Group, INC.

- These cost estimates do not account for median plantings, turf, or hardscaping.
- Tree costs do not include labor fees

Funding Sources

SALES TAX FUNDS THROUGH PIAC

The Public Improvement Advisory Committee (PIAC) is a group that solicits input from the general citizenry on how the citywide and neighborhood portions of the capital budget should be spent. This committee then submits a five-year capital improvements and neighborhood recommendations budget to the City Council for their consideration.

TAX INCREMENT FINANCING (TIF)

TIF is used to finance public improvements through the increase in future property tax dollars. The idea being if the public improvement is made, then new or redevelopment will occur raising the value of the land allowing the City to collect more property tax on that land. The additional property tax is earmarked to pay for the improvements over a period of several years.

TRANSPORTATION DEVELOPMENT DISTRICT (TDD)

TDDs are formed to fund, promote, plan, design, construct, improve, maintain, and operate one or more transportation projects or to assist in such activity. Once formed, TDDs become a separate political subdivision with an elected board of directors for the district, with the ability to condemn property, contract with parties, lease or purchase real and personal property, and sue or be sued in a court of law.

SPECIAL BUSINESS DISTRICTS (SBD)

A SBD grants an area the ability to levy special fees and taxes within the district to pay for public improvement projects within the district. Discretion as to the type and amount of the expenditures lies with the local government which appoints an advisory committee to make recommendations. The district may also issue general obligation bonds for up to 20 years, authorized by the City and approved by the voters within the district. These bonds do count against the City's debt limit.

COMMUNITY IMPROVEMENT DISTRICT (CID)

CIDs may be formed as either a political subdivision or a non-profit organization. The purpose of CIDs is to raise money for public improvement projects for a specific area. They may use special assessment fees to fund projects and those that are organized as political subdivisions can be funded through a levy on real property tax.

COMMUNITY DEVELOPMENT BLOCK GRANTS (CDBG)

Projects seeking these grants must meet at least one of three federal guidelines: the project must benefit low and/or moderate income persons; prevent or eliminate slums and blight; or meet an urgent community development need. CDBG funds have been a catalyst for redevelopment and improvement in many blight and substandard areas.



THE SAFE, ACCOUNTABLE, FLEXIBLE, AND EFFICIENT TRANSPORTATION EQUITY ACT (SAFETEA)

This is the major federal source of funds for transportation projects throughout the country. There are numerous different programs within the scope of this Act that could benefit the Knobtown Area. The two most prevalent are the Transportation Enhancement Program, which provides funding for transportation related urban design enhancements, and the Recreational Trails Program, that is responsible for funding the construction of trails.

STORMWATER MANAGEMENT DISTRICT

A city may establish a special impact fee to fund stormwater improvement projects from new development, current property owners, or future property owners. A district can be established for all, or a portion of a watershed such as the Little Blue River Valley and would serve as a fee supported entity to address stormwater management needs.

DEBT FINANCING

Typically reserved for capital-intensive projects, local governments can issue debt to finance stormwater projects. Revenue bonds based on assessments or utility fees may be used, or general obligation bonds backed by the credit of the municipality and based on revenue generated through taxes and other fees.

FEDERAL, STATE, OR REGIONAL GRANTS OR LOANS

The Missouri Department of Natural Resources issues a 319 grant, which provides money administered by EPA. The goal of this grant is to improve the quality of Missouri's streams for non-point source pollution. This grant is based on a watershed-approach and would require the cooperation of the other communities in the watershed.